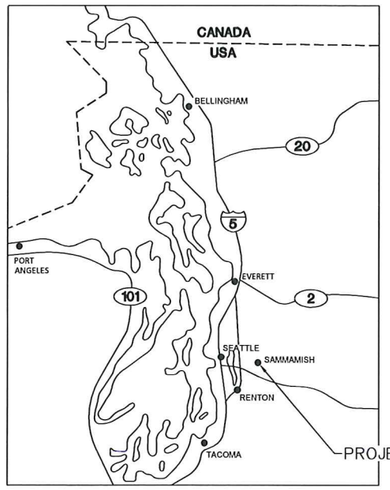
2018 INTERSECTION IMPROVEMENTS PROJECT

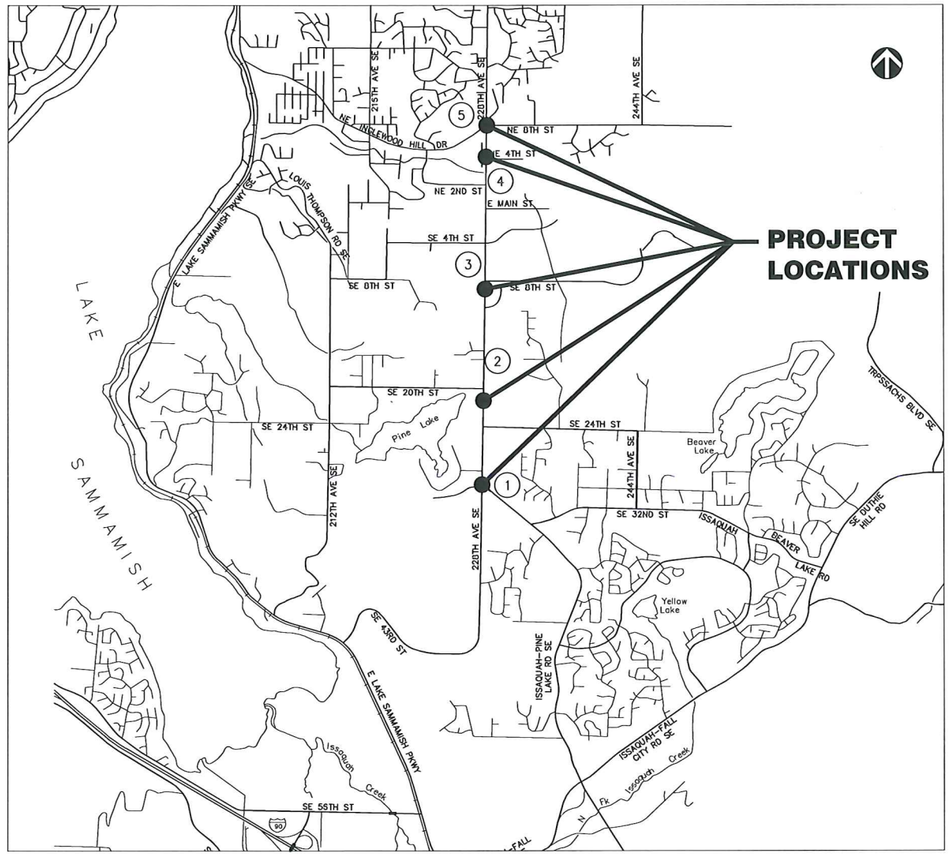
LOCATION MAP

NOT TO SCALE



VICINITY MAP

NOT TO SCALE



SHEET INDEX

TITLE	SHEET NO.
COVER	1
STANDARD NOTES	2–3
SITE PREP AND T.E.S.C. PLAN	4–8
ROADWAY PLANS	9–13
TYPICAL SECTIONS AND DETAILS	14
TRAFFIC CONTROL PLANS	15–17

CITY OFFICIALS

MAYOR: CHRISTIE MALCHOW

DEPUTY MAYOR: KAREN MORAN

COUNCIL MEMBERS: TOM HORNISH
JASON RITCHIE
CHRIS ROSS
PAMELA STUART
RAMIRO VALDERRAMA

CITY MANAGER: LYMAN HOWARD

DIRECTOR OF PUBLIC WORKS: STEVE LENISZEWSKI, P.E.

CITY ENGINEER: ANDREW ZAGARS, P.E.

PROJECT/TRAFFIC ENGINEER: STEVEN CHEN, P.E.

PHONE NUMBERS

FIRE / MEDIC 1 / POLICE	911
SAMMAMISH PUBLIC WORKS	(425) 295–0500
WATER (SAMMAMISH PLATEAU WATER & SEWER DISTRICT)	(425) 392–6256
SEWER (SAMMAMISH PLATEAU WATER & SEWER DISTRICT)	(425) 392–6256
POWER (PUGET SOUND ENERGY)	(425) 417–9188
GAS (PUGET SOUND ENERGY)	(425) 417–9188
COMMUNICATIONS (COMCAST)	(425) 263–5348
COMMUNICATIONS (CENTURY LINK)	(206) 261–1402
U.S. POST OFFICE, ISSAQUAH POSTMASTER	(425) 837–8795
UNDERGROUND UTILITY LOCATES	811

APPROVED BY:

PROJECT ENGINEER: _____ DATE: 6/20/18

CITY ENGINEER: _____ DATE: 6/21/18

DIRECTOR OF PUBLIC WORKS: for Cheryl Paston DATE: 6/21/18

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. ROND — STATE OF WASHINGTON — 23813 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

COVER

CV01

1 OF 17

By: RAKO Date: 2:29 PM File:P:\C\COSA0000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTCV001COSA0023_INT.dwg
Layout: CV01

GENERAL SITE PLAN NOTES

1. ALL DESIGN AND CONSTRUCTION SHALL BE IN ACCORDANCE WITH PERMIT CONDITIONS, THE SAMMAMISH MUNICIPAL CODE (SMC), THE SAMMAMISH PUBLIC WORKS STANDARDS (PWS) AND THE CONDITIONS OF APPROVAL. IT SHALL BE THE SOLE RESPONSIBILITY OF THE APPLICANT (CITY ENGINEER) AND THE CONTRACTOR TO CORRECT ANY ERROR, OMISSION, OR DEVIATION FROM THE ABOVE REQUIREMENTS FOUND IN THESE PLANS.
2. THE DESIGN ELEMENTS WITHIN THESE PLANS HAVE BEEN REVIEWED ACCORDING TO THE CITY OF SAMMAMISH PUBLIC WORKS DEVELOPMENT REVIEW CHECKLIST. ANY DEVIATION FROM ADOPTED STANDARDS IS NOT ALLOWED UNLESS SPECIFICALLY APPROVED BY THE CITY IN WRITING PRIOR TO CONSTRUCTION.
3. APPROVAL OF THIS PLAN DOES NOT CONSTITUTE AN APPROVAL OF UTILITIES NOT OWNED BY THE CITY (E.G. DOMESTIC WATER CONVEYANCE, SEWER CONVEYANCE, GAS, ELECTRICAL, ETC.).
4. PRIOR TO ANY CONSTRUCTION OR DEVELOPMENT ACTIVITY, A PRECONSTRUCTION MEETING SHALL BE HELD BETWEEN THE CITY OF SAMMAMISH, THE APPLICANT(S), AND THE APPLICANT'S CONSTRUCTION REPRESENTATIVE.
5. A COPY OF THESE APPROVED PLANS SHALL BE ON THE JOB SITE WHENEVER CONSTRUCTION IS IN PROGRESS.
6. CONSTRUCTION HOURS ARE 7:00 AM TO 8:00 PM MONDAY THROUGH FRIDAY AND 9:00 AM TO 6:00 PM ON SATURDAYS. WORK IS NOT ALLOWED ON SUNDAYS AND SOME HOLIDAYS IN ACCORDANCE WITH SMC 16.05.030. WORK IN THE PUBLIC RIGHT-OF-WAY IS FURTHER RESTRICTED BY THE SPECIAL PROVISIONS PART OF THE CONTRACT PROVISIONS.
7. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY POSSESSION OF ALL NECESSARY CONSTRUCTION EASEMENTS BEFORE INITIATING ANY OFF-SITE WORK.
8. VERTICAL DATUM SHALL BE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD 1988) UNLESS OTHERWISE APPROVED BY THE CITY OF SAMMAMISH. HORIZONTAL DATUM SHALL BE IN THE WASHINGTON STATE PLANE COORDINATE SYSTEM, NORTH ZONE, USING NORTH AMERICAN DATUM OF 1983 (NAD 83 (1991)) UNLESS OTHERWISE APPROVED BY THE CITY.
9. DEWATERING (GROUNDWATER) SYSTEM CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CURRENT WSDOT STANDARD SPECIFICATIONS.
10. OPEN CUTTING OF ROADWAYS IS NOT ALLOWED UNLESS SPECIFICALLY APPROVED BY THE CITY AND NOTED ON THESE APPROVED PLANS.
11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING ADEQUATE SAFEGUARDS, SAFETY DEVICES, PROTECTIVE EQUIPMENT, FLAGGERS, AND ANY OTHER NEEDED ACTIONS TO PROTECT THE LIFE, HEALTH, AND SAFETY OF THE PUBLIC, AND TO PROTECT PROPERTY IN CONNECTION WITH THE PERFORMANCE OF WORK COVERED BY THE CONTRACTOR. ANY WORK WITHIN THE TRAVELED RIGHT-OF-WAY THAT MAY INTERRUPT NORMAL TRAFFIC FLOW SHALL REQUIRE AT LEAST ONE FLAGGER FOR EACH LANE OF TRAFFIC AFFECTED. MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) SHALL APPLY. WORK IN RIGHT-OF-WAY IS NOT AUTHORIZED UNTIL A TRAFFIC CONTROL PLAN IS APPROVED BY THE CITY.
12. ANY CHANGES TO THE APPROVED PLANS MUST BE SUBMITTED TO THE CITY IN WRITING. NO CONSTRUCTION ON THESE CHANGES SHALL BEGIN UNTIL APPROVED BY THE CITY.
13. PER RCW SECTION 19.122, CALL 811 BETWEEN TEN (10) AND TWO (2) BUSINESS DAYS BEFORE BEGINNING EXCAVATION WHERE ANY UNDERGROUND UTILITIES MAY BE LOCATED. FAILURE TO DO SO COULD RESULT IN THE CONTRACTOR BEARING SUBSTANTIAL REPAIR COSTS.
14. APPROXIMATE LOCATIONS OF EXISTING UTILITIES HAVE BEEN OBTAINED FROM AVAILABLE RECORDS AND ARE SHOWN FOR CONVENIENCE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFICATION OF EXISTING UTILITY LOCATIONS WHETHER OR NOT THESE UTILITIES ARE SHOWN ON THE PLANS. THE CONTRACTOR SHALL EXERCISE ALL CARE TO AVOID DAMAGE TO ANY UTILITY. IF CONFLICTS WITH EXISTING UTILITIES ARISE DURING CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE CITY PUBLIC WORKS CONSTRUCTION INSPECTOR AND ANY CHANGES REQUIRED SHALL BE APPROVED BY THE CITY OF SAMMAMISH PUBLIC WORKS DEPARTMENT PRIOR TO COMMENCEMENT OF RELATED CONSTRUCTION ON THE PROJECT. THE CONTRACTOR IS RESPONSIBLE TO ENSURE THAT UTILITY LOCATES ARE MAINTAINED THROUGHOUT THE LIFE OF THE PROJECT.
15. ALL DAMAGES INCURRED TO PUBLIC AND/OR PRIVATE PROPERTY BY THE CONTRACTOR DURING THE COURSE OF CONSTRUCTION SHALL BE PROMPTLY REPAIRED TO THE SATISFACTION OF THE PUBLIC WORKS CONSTRUCTION INSPECTOR BEFORE PROJECT APPROVAL AND/OR THE RELEASE OF THE PROJECT'S PERFORMANCE BOND.
16. ALL LANDSCAPED AREAS OF THE PROJECT SHALL INCLUDE A MINIMUM OF 8-INCHES OF COMPOSTED SOIL AMENDMENT ATOP A MINIMUM OF 4-INCHES SCARIFIED SOIL.
17. NO FINAL CUT OR FILL SLOPE SHALL EXCEED SLOPES OF TWO (2) HORIZONTAL TO ONE (1) VERTICAL WITHOUT STABILIZATION BY ROCKERY OR BY A STRUCTURAL RETAINING WALL, UNLESS DESIGNED AND COMPLETED UNDER THE SUPERVISION OF A LICENSED GEOTECHNICAL ENGINEER.
18. THESE PLANS ARE APPROVED FOR STANDARD ROAD AND DRAINAGE IMPROVEMENTS ONLY. STRUCTURES SUCH AS BRIDGES, VAULTS, AND RETAINING WALLS REQUIRE ADDITIONAL PERMITS FROM THE CITY PRIOR TO CONSTRUCTION.
19. NO MATERIALS OR EQUIPMENT SHALL BE PLACED OR STORED ON PUBLIC RIGHT-OF-WAY AT ANY TIME WITHOUT THE ENGINEER'S APPROVAL.
20. NOT USED
21. CONSTRUCTION NOISE SHALL BE LIMITED TO THE CONSTRUCTION HOURS AS STATED IN SMC 16.05.030.

ESC PLAN NOTES

1. APPROVAL OF THIS ESC PLAN DOES NOT CONSTITUTE AN APPROVAL OF PERMANENT ROAD OR DRAINAGE DESIGN (E.G., SIZE AND LOCATION OF ROADS, PIPES, RESTRICTORS, CHANNELS, RETENTION FACILITIES, UTILITIES, ETC.).
2. THE IMPLEMENTATION OF THIS ESC PLAN AND THE CONSTRUCTION, MAINTENANCE, REPLACEMENT, AND UPGRADING OF THESE ESC FACILITIES IS THE RESPONSIBILITY OF THE APPLICANT/ESC SUPERVISOR UNTIL ALL CONSTRUCTION IS APPROVED.
3. THE BOUNDARIES OF THE CLEARING LIMITS SHOWN ON THIS PLAN SHALL BE CLEARLY FLAGGED BY SURVEY TAPE OR FENCING, PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, DISTURBANCE BEYOND THE CLEARING LIMITS IS NOT PERMITTED. THE CLEARING LIMITS SHALL BE MAINTAINED BY THE APPLICANT/ESC SUPERVISOR FOR THE DURATION OF CONSTRUCTION.
4. STABILIZED CONSTRUCTION ENTRANCES SHALL BE INSTALLED AT THE BEGINNING OF CONSTRUCTION AND MAINTAINED FOR THE DURATION OF THE PROJECT. ADDITIONAL MEASURES, SUCH AS CONSTRUCTED WHEEL WASH SYSTEMS OR WASH PADS, MAY BE REQUIRED (AS DIRECTED BY THE CITY ENGINEER OR RESIDENT INSPECTOR) TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN AND TRACK OUT TO ROAD RIGHT-OF-WAY DOES NOT OCCUR FOR THE DURATION OF THE PROJECT.
5. THE ESC FACILITIES SHOWN ON THIS PLAN MUST BE CONSTRUCTED PRIOR TO OR IN CONJUNCTION WITH ALL CLEARING AND GRADING SO AS TO ENSURE THAT THE TRANSPORT OF SEDIMENT TO SURFACE WATERS, DRAINAGE SYSTEMS, FLOW CONTROL BMP LOCATIONS (EXISTING AND PROPOSED), AND ADJACENT PROPERTIES IS MINIMIZED.
6. THE ESC FACILITIES SHOWN ON THIS PLAN ARE THE MINIMUM REQUIREMENTS FOR ANTICIPATED SITE CONDITIONS. DURING THE CONSTRUCTION PERIOD, THESE ESC FACILITIES SHALL BE UPGRADED AS NEEDED FOR UNEXPECTED STORM EVENTS AND MODIFIED TO ACCOUNT FOR CHANGING SITE CONDITIONS (E.G., ADDITIONAL COVER MEASURES, ADDITIONAL SUMP PUMPS, RELOCATION OF DITCHES AND SILT FENCES, PERIMETER PROTECTION ETC.) OR AS DIRECTED BY THE CITY.
7. THE ESC FACILITIES SHALL BE INSPECTED DAILY BY THE APPLICANT/ESC SUPERVISOR DURING NON-RAINFALL PERIODS, EVERY HOUR (DAYLIGHT) DURING A RAINFALL EVENT, AND AT THE END OF EVERY RAINFALL, AND MAINTAINED TO ENSURE THEIR CONTINUED PROPER FUNCTIONING. IN ADDITION, TEMPORARY SILTATION PONDS AND ALL TEMPORARY SILTATION CONTROLS SHALL BE MAINTAINED IN A SATISFACTORY CONDITION UNTIL SUCH TIME THAT CLEARING AND/OR CONSTRUCTION IS COMPLETED, PERMANENT DRAINAGE FACILITIES ARE OPERATIONAL, AND THE POTENTIAL FOR EROSION HAS PASSED. WRITTEN RECORDS SHALL BE KEPT OF WEEKLY REVIEWS OF THE ESC FACILITIES DURING THE WET SEASON (OCT. 1 TO APRIL 30) AND OF MONTHLY REVIEWS DURING THE DRY SEASON (MAY 1 TO SEPT 30).
8. ANY AREAS OF EXPOSED SOILS, INCLUDING ROADWAY EMBANKMENTS, THAT WILL NOT BE DISTURBED FOR TWO CONSECUTIVE DAYS DURING THE WET SEASON OR SEVEN DAYS DURING THE DRY SEASON SHALL BE IMMEDIATELY STABILIZED WITH THE APPROVED ESC COVER METHODS (E.G., SEEDING, MULCHING, PLASTIC COVERING, ETC.).
9. ANY AREA NEEDING ESC MEASURES THAT DO NOT REQUIRE IMMEDIATE ATTENTION SHALL BE ADDRESSED WITHIN SEVEN (7) DAYS.
10. THE ESC FACILITIES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH (MORE FREQUENTLY AS REQUIRED BY THE PUBLIC WORKS CONSTRUCTION INSPECTOR) OR WITHIN TWENTY-FOUR (24) HOURS FOLLOWING A STORM EVENT.
11. AT NO TIME SHALL MORE THAN ONE (1) FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT-LADEN WATER INTO THE DOWNSTREAM SYSTEM.
12. ANY PERMANENT RETENTION/DETENTION FACILITY USED AS A TEMPORARY SETTLING BASIN SHALL BE MODIFIED WITH THE NECESSARY EROSION CONTROL MEASURES AND SHALL PROVIDE ADEQUATE STORAGE CAPACITY. IF THE FACILITY IS TO FUNCTION ULTIMATELY AS AN INFILTRATION SYSTEM, THE PERMANENT FACILITY SHALL NOT BE USED AS A TEMPORARY SETTLING BASIN, ELSE THE TEMPORARY FACILITY MUST BE GRADED SO THAT THE BOTTOM AND SIDES ARE AT LEAST THREE FEET ABOVE THE FINAL GRADE OF THE PERMANENT FACILITY. NO UNDERGROUND DETENTION TANK, DETENTION VAULT, OR SYSTEM WHICH BACKS UNDER OR INTO A POND SHALL BE USED AS A TEMPORARY SETTLING BASIN. FLOW CONTROL BMP AREAS (EXISTING OR PROPOSED) SHALL NOT BE USED AS TEMPORARY FACILITIES AND SHALL BE PROTECTED FROM SEDIMENTATION AND INTRUSION.
13. COVER MEASURES WILL BE APPLIED IN CONFORMANCE WITH APPENDIX D OF THE KING COUNTY SURFACE WATER DESIGN MANUAL.
14. PRIOR TO THE BEGINNING OF THE WET SEASON (OCTOBER 1) OF EACH YEAR, ALL DISTURBED AREAS SHALL BE REVIEWED TO IDENTIFY WHICH ONES CAN BE SEEDED IN PREPARATION FOR THE WINTER RAINS. THE IDENTIFIED DISTURBED AREA SHALL BE SEEDED WITHIN ONE WEEK AFTER OCTOBER 1. A SKETCH MAP DEPICTING THE AREAS TO BE SEEDED AND THE AREAS TO REMAIN UNCOVERED SHALL BE SUBMITTED TO THE PUBLIC WORKS CONSTRUCTION INSPECTOR. THE INSPECTOR MAY REQUIRE SEEDING OF ADDITIONAL AREAS IN ORDER TO PROTECT SURFACE WATERS, ADJACENT PROPERTIES, OR DRAINAGE FACILITIES.
15. NOT USED
16. ALL EROSION/SEDIMENTATION CONTROL PONDS WITH A DEAD STORAGE DEPTH EXCEEDING SIX INCHES (6") MUST HAVE A HIGHLY VISIBLE PERIMETER FENCE WITH A MINIMUM HEIGHT OF THREE FEET (3').
17. NOT USED
18. CLEARING LIMITS SHALL BE DELINEATED WITH A CLEARING CONTROL FENCE. THE CLEARING CONTROL FENCE SHALL CONSIST OF A FOUR-FOOT (4') HIGH TEMPORARY CONSTRUCTION FENCE. CLEARING CONTROL FENCES ALONG WETLAND OR STREAM BUFFERS OR UPSLOPE OF SENSITIVE SLOPES SHALL BE ACCOMPANIED BY TWO ROWS OF EROSION CONTROL FENCE. IF DETERMINED APPROPRIATE BY CITY OF SAMMAMISH A SIX-FOOT (6') HIGH CHAIN LINK FENCE MAY BE REQUIRED.

ESC PLAN NOTES (CONTINUED)

19. IF SEDIMENT IS TRACKED OFFSITE, PUBLIC ROADS SHALL BE CLEANED THOROUGHLY AT THE END OF EACH DAY, OR MORE FREQUENTLY DURING WET WEATHER, IF NECESSARY TO PREVENT SEDIMENT FROM ENTERING WATERS OF THE STATE. SEDIMENT SHALL BE REMOVED FROM ROADS BY SHOVELING OR PICKUP SWEEPING AND SHALL BE TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA. STREET WASHING WILL BE ALLOWED ONLY AFTER SEDIMENT IS REMOVED IN THIS MANNER. STREET WASH WASTEWATER SHALL BE CONTROLLED BY PUMPING BACK ONSITE, OR OTHERWISE BE PREVENTED FROM DISCHARGING INTO DRAINAGE SYSTEMS TRIBUTARY TO SURFACE WATERS.
20. ANY CATCH BASINS COLLECTING RUNOFF FROM THE SITE, WHETHER THEY ARE ON OR OFF THE SITE, SHALL HAVE THEIR GRATES COVERED WITH FILTER FABRIC DURING CONSTRUCTION. CATCH BASINS DIRECTLY DOWNSTREAM OF THE CONSTRUCTION ENTRANCE OR ANY OTHER CATCH BASIN AS DETERMINED BY THE PUBLIC WORKS CONSTRUCTION INSPECTOR SHALL BE PROTECTED WITH A "FILTER FABRIC SOCK" OR EQUIVALENT. AT NO TIME SHALL MORE SEDIMENT THAN ONE-THIRD (1/3) OF THE AVAILABLE STORAGE BE ALLOWED TO ACCUMULATE WITHIN A CATCH BASIN INSERT. SEE SECTION D.2.1.5.3 OF THE 2016 KCSWDM APPENDIX D.
21. THE WASHED GRAVEL BACKFILL ADJACENT TO THE FILTER FABRIC FENCE SHALL BE REPLACED AND THE FILTER FABRIC CLEANED IF IT IS NONFUNCTIONAL BY EXCESSIVE SILT ACCUMULATION AS DETERMINED BY THE CITY OF SAMMAMISH PUBLIC WORKS CONSTRUCTION INSPECTOR. ALL INTERCEPTOR SWALES SHALL BE CLEANED IF SILT ACCUMULATION EXCEEDS ONE-HALF FOOT (0.5') DEPTH.
22. ROCK FOR EROSION PROTECTION OF ROADWAY DITCHES, WHERE REQUIRED, MUST BE OF SOUND QUARRY ROCK, PLACED TO A DEPTH OF 1' AND MUST MEET WSDOT SPECIFICATIONS 4"-8" ROCK/40%-70% PASSING; 2"-4" ROCK/30%-40% PASSING; AND 1"-2" ROCK/10%-20% PASSING.
23. FLUSHING CONCRETE BY-PRODUCTS OR TRUCKS NEAR OR INTO THE STORM DRAINAGE SYSTEM SHALL NOT BE ALLOWED. IF EXPOSED AGGREGATE IS FLUSHED INTO THE STORM SYSTEM, IT MAY RESULT IN RE-INSPECTION AND RE-CLEANING THE ENTIRE AFFECTED DOWNSTREAM STORM SYSTEM, OR POSSIBLY RE-LAYING THE STORM LINE.
24. MAXIMUM RELEASE RATE FROM THE SITE AT ANY TIME DURING CONSTRUCTION AND DURING THE MAINTENANCE AND DEFECT PERIOD SHALL BE NO MORE THAN ONE-HALF OF THE 2-YEAR PEAK FLOW WHEN THE FLOW CONTROL STRUCTURE IS BYPASSED.
25. DURING THE WET SEASON (OCTOBER 1 – APRIL 30) NOTES:
    a. THE ALLOWED TIME THAT A DISTURBED AREA MAY REMAIN UNWORKED WITHOUT COVER MEASURES IS REDUCED TO TWO CONSECUTIVE WORKING DAYS, RATHER THAN SEVEN (SECTION D.2.1.2).
    b. STOCKPILES AND STEEP CUT AND FILL SLOPES ARE TO BE PROTECTED IF UNWORKED FOR MORE THAN 12 HOURS (SECTION D.2.1.2).
    c. COVER MATERIALS SUFFICIENT TO COVER ALL DISTURBED AREAS SHALL BE STOCKPILED ON SITE (SECTION D.2.1.2).
    d. ALL AREAS THAT ARE TO BE UNWORKED DURING THE WET SEASON SHALL BE SEEDED WITHIN ONE WEEK OF THE BEGINNING OF THE WET SEASON (SECTION D.2.1.2.6).
    e. MULCH IS REQUIRED TO PROTECT ALL SEEDED AREAS (SECTION D.2.1.2.2).
    f. FIFTY LINEAR FEET OF SILT FENCE (AND THE NECESSARY STAKES) PER ACRE OF DISTURBANCE MUST BE STOCKPILED ON SITE (SECTION D.2.1.3.1).
    g. CONSTRUCTION ROAD AND PARKING LOT STABILIZATION ARE REQUIRED FOR ALL SITES UNLESS THE SITE IS UNDERLAIN BY COARSE-GRAINED SOIL (SECTION D.2.1.4.2).
    h. SEDIMENT RETENTION IS REQUIRED UNLESS NO OFFSITE DISCHARGE IS ANTICIPATED FOR THE SPECIFIED DESIGN FLOW (SECTION D.2.1.5).
    i. SURFACE WATER CONTROLS ARE REQUIRED UNLESS NO OFFSITE DISCHARGE IS ANTICIPATED FOR THE SPECIFIED DESIGN FLOW (SECTION D.2.1.6).
    j. PHASING AND MORE CONSERVATIVE BMPS MUST BE EVALUATED FOR CONSTRUCTION ACTIVITY NEAR SURFACE WATERS (SECTION D.2.4.3).
    k. ANY RUNOFF GENERATED BY DEWATERING MAY BE REQUIRED TO DISCHARGE TO THE SANITARY SEWER (WITH APPROPRIATE DISCHARGE AUTHORIZATION), PORTABLE SAND FILTER SYSTEMS, OR HOLDING TANKS (SECTION D.2.2).
    l. WHEN LOCATED WITHIN AN ENVIRONMENTALLY CRITICAL AREA, A WET SEASON PERMIT IS REQUIRED.
26. A DETAILED CONSTRUCTION SEQUENCE IS REQUIRED TO ENSURE THAT EROSION AND SEDIMENT CONTROL MEASURES ARE APPLIED AT THE APPROPRIATE TIMES. A CONSTRUCTION SEQUENCE TEMPLATE IS PROVIDED BELOW, TO BE UPDATED TO SPECIFICALLY MATCH THE PROJECT:
    a. PRE-CONSTRUCTION MEETING.
    b. POST SIGN WITH NAME AND PHONE NUMBER OF CSWPP/ESC SUPERVISOR.
    c. FLAG OR FENCE CLEARING LIMITS.
    d. INSTALL CATCH BASIN PROTECTION, IF REQUIRED.
    e. GRADE AND INSTALL CONSTRUCTION ENTRANCE(S).
    f. INSTALL PERIMETER PROTECTION (SILT FENCE, BRUSH BARRIER, ETC.).
    g. CONSTRUCT SEDIMENT PONDS AND TRAPS.
    h. GRADE AND STABILIZE CONSTRUCTION ROADS.
    i. CONSTRUCT SURFACE WATER CONTROLS (INTERCEPTOR DIKES, PIPE SLOPE DRAINS, ETC.) SIMULTANEOUSLY WITH CLEARING AND GRADING FOR PROJECT DEVELOPMENT.
    j. MAINTAIN EROSION CONTROL MEASURE IN ACCORDANCE WITH CITY PUBLIC WORKS STANDARDS AND MANUFACTURER'S RECOMMENDATIONS.
    k. RELOCATE EROSION CONTROL MEASURES OR INSTALL NEW MEASURES SO THAT AS SITE CONDITIONS CHANGE, THE EROSION AND SEDIMENT CONTROL IS ALWAYS IN ACCORDANCE WITH THE CITY ESC MINIMUM REQUIREMENTS.
    l. COVER ALL AREAS WITHIN THE SPECIFIED TIME FRAME WITH STRAW, WOOD FIBER MULCH, COMPOST, PLASTIC SHEETING, CRUSHED ROCK OR EQUIVALENT.
    m. STABILIZE ALL AREAS THAT REACH FINAL GRADE WITHIN SEVEN (7) DAYS.
    n. SEED OR SOD ANY AREAS TO REMAIN UN-WORKED FOR MORE THAN THIRTY (30) DAYS.
    o. UPON COMPLETION OF THE PROJECT, ALL DISTURBED AREAS MUST BE STABILIZED AND BEST MANAGEMENT PRACTICES (BMPS) REMOVED IF APPROPRIATE.

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. WOLFE STATE OF WASHINGTON 22913 REGISTERED PROFESSIONAL ENGINEER 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

SN01

STANDARD NOTES

ROADWAY PLAN NOTES

1. ALL CONCRETE FOR SIDEWALKS AND CURB AND GUTTER MUST BE 4,000–PSI MINIMUM AND FOUR (4) INCHES THICK WHEN NOT VEHICLE ACCESSIBLE AND SIX (6) INCHES THICK WHEN ACCESSIBLE TO VEHICLES OR EIGHT (8) INCHES THICK IN COMMERCIAL DRIVEWAY APPROACHES.
2. IN THE CASE OF NEW ROAD CONSTRUCTION OR RECONSTRUCTION REQUIRING MAILBOXES TO BE MOVED OR REARRANGED, THE APPLICANT/CONTRACTOR SHALL COORDINATE WITH THE U.S. POSTAL SERVICE FOR THE NEW LOCATION OF THE MAILBOX STRUCTURE, AND SHALL NOTIFY THE CITY PUBLIC WORKS CONSTRUCTION INSPECTOR AND MAILBOX USER(S) OF THE CHANGE A MINIMUM OF TWO (2) WEEKS BEFORE IT OCCURS.
3. ANY ROADWAY SIGNAGE OR STRIPING THAT IS DAMAGED, REMOVED, OR TEMPORARILY RELOCATED BY THE CONTRACTOR SHALL BE RESTORED TO MEET THE CURRENT CITY OF SAMMAMISH PUBLIC WORKS STANDARDS.
4. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ADEQUATE TEMPORARY TRAFFIC CONTROL TO ENSURE TRAFFIC SAFETY DURING CONSTRUCTION ACTIVITIES. THEREFORE, THE CONTRACTOR SHALL SUBMIT A TRAFFIC CONTROL PLAN TO THE CITY PUBLIC WORKS CONSTRUCTION INSPECTOR AT LEAST 48 HOURS PRIOR TO STARTING ANY WORK IN THE RIGHT–OF–WAY. ALL TRAFFIC CONTROL DEVICES SHALL CONFORM TO THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (MUTCD) OR AS APPROVED BY THE TRAFFIC ENGINEER.
5. WHERE A SIDEWALK IS TO BE CONSTRUCTED ABOVE A SLOPE OR ADJACENT TO A ROCKERY OR RETAINING WALL WHERE THE LOWEST FINISHED ELEVATION OF THE SLOPE, ROCKERY, OR RETAINING WALL IS TO BE THIRTY INCHES (30") OR MORE BELOW THE FINISHED ELEVATION OF THE SIDEWALK, A SAFETY RAILING SHALL BE REQUIRED WHEN: (A) THE VERTICAL WALL FACE IS LESS THAN FOUR FEET IN HORIZONTAL DISTANCE FROM THE NEAR SIDE FACE OF THE FACILITY; (B) THE VERTICAL WALL FACE IS GREATER THAN FOUR FEET HORIZONTALLY TO THE NEAR SIDE FACE OF THE FACILITY AND THE SLOPE TO THE WALL IS STEEPER THAN 1V:3H; (C) THE SLOPES ADJACENT TO THE FACILITY AVERAGE GREATER THAN 1V:2H. SEE FIGURE 15.3 OF THE PUBLIC WORKS STANDARDS.
6. NOT USED
7. SIDEWALK AND CURB AND GUTTER CANNOT BE POURED MONOLITHICALLY. THERE MUST BE A FULL DEPTH EXPANSION JOINT BETWEEN THEM.
8. NOT USED
9. WHEN AN EXISTING ROADWAY IS TO RECEIVE A HALF–STREET OVERLAY, THE EXISTING ROADWAY MUST BE COLD PLANED AT THE EDGE OF THE GUTTER AND CENTERLINE. WHEN THE EXISTING ROADWAY IS TO RECEIVE A FULL–STREET OVERLAY, IT MUST BE COLD PLANED FOR THE FULL WIDTH OF THE ROADWAY.
10. ALL NEW CHANNELIZATION AND SIGNAGE SHALL BE PROVIDED AND LAID OUT CONSISTENT WITH THE CITY OF SAMMAMISH PUBLIC WORKS TRAFFIC ENGINEER APPROVAL. CONTACT THE CITY TRAFFIC ENGINEER AT LEAST ONE (1) WEEK PRIOR TO SCHEDULING CHANNELIZATION.
11. ALL NEW SIGNS REQUIRED IN THE PUBLIC RIGHT–OF–WAY MUST BE INSTALLED BY THE CONTRACTOR PER CITY OF SAMMAMISH PUBLIC WORKS STANDARDS. TO INITIATE SIGNAGE INSTALLATION, CONTRACTOR SHALL CONTACT THE PUBLIC WORKS INSPECTOR A MINIMUM OF SIX (6) WEEKS PRIOR TO FINAL ACCEPTANCE.
12. WHEN INSTALLING NEW SIDEWALK, THE AREA BEHIND THE SIDEWALK MUST BE GRADED SO THAT SURFACE WATER DOES NOT DRAIN OVER THE SIDEWALK.
13. NOT USED
14. OPEN CUT ROAD CROSSINGS FOR UTILITY TRENCHES ON EXISTING TRAVELED ROADWAY SHALL BE BACKFILLED ONLY WITH ⅝" MINUS CRUSHED ROCK AND MECHANICALLY COMPACTED (UNLESS OTHERWISE APPROVED BY THE CITY). FOR STREETS CLASSIFIED AS ARTERIALS, BACKFILL FOR CROSSINGS SHALL BE CDF. CUTS INTO THE EXISTING ASPHALT SHALL BE NEAT LINE CUT WITH SAW OR JACKHAMMER IN A CONTINUOUS LINE. A TEMPORARY COLD MIX PATCH MUST BE PLACED IMMEDIATELY AFTER BACKFILL AND COMPACTION. A PERMANENT HOT MIX PATCH SHALL BE PLACED WITHIN 30 DAYS AND SHALL BE A MINIMUM OF 1" THICKER THAN THE ORIGINAL ASPHALT WITH A MINIMUM THICKNESS OF 2"
15. ALL TRENCH BACKFILL SHALL BE COMPACTED TO 95 PERCENT DENSITY (MODIFIED PROCTOR ASTM–D1557) IN ROADWAYS, ROADWAY SHOULDERS, ROADWAY PRISM AND DRIVEWAYS, AND 90 PERCENT DENSITY (MODIFIED PROCTOR ASTM–D1557) IN UNPAVED AREAS. ALL PIPE ZONE COMPACTION SHALL BE 95 PERCENT (MODIFIED PROCTOR ASTM–D1557).
16. WHEN CONSTRUCTING NEW CURB AND GUTTER THAT DOES NOT ALIGN WITH THE EXISTING EDGE OF PAVEMENT, THE ROADWAY MUST BE TAPERED AND SHALL MEET THE CURRENT CITY PUBLIC WORKS STANDARDS.
17. WHEN AN EXISTING ROADWAY IS TO BE WIDENED, THE EXISTING PAVEMENT MUST BE SAW CUT AT LEAST ONE FOOT FROM THE EDGE TO PROVIDE A PROPER MATCH BETWEEN NEW AND EXISTING ASPHALT. WHEN THE EXISTING PAVEMENT CONDITION PREVENTS A STRAIGHT CUT, THE SAW CUT MUST BE MADE AT THE NEAREST LANE EDGE. ALL SAW CUTS SHALL BE PARALLEL OR PERPENDICULAR TO THE RIGHT–OF–WAY CENTERLINE.
18. ALL PEDESTRIAN ACCESS AREAS INCLUDING SIDEWALKS AND SIDEWALK RAMPS SHALL BE CONSISTENT WITH CURRENT ADA REQUIREMENTS. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ENSURE ALL PEDESTRIAN ACCESS MEET CURRENT ADA STANDARDS. WHEN THIS CANNOT BE MET, THE ENGINEER SHALL BE NOTIFIED AND THE ENGINEER WILL PREPARE MAXIMUM EXTENT FEASIBLE (MEF) DOCUMENTATION TO BE REFERENCED WITH THE AS–BUILT DRAWINGS.
19. PROOF ROLLING SHALL BE REQUIRED OF ALL SIDEWALKS, CURBS, AND ROADWAYS AT THE DISCRETION OF THE CITY PUBLIC WORKS CONSTRUCTION INSPECTOR TO ENSURE ADEQUATE COMPACTION.

SWPPS PLAN NOTES

1. ALL POLLUTANTS, INCLUDING WASTE MATERIALS, THAT OCCUR ONSITE SHALL BE HANDLED AND DISPOSED OF IN A MANNER THAT DOES NOT CAUSE CONTAMINATION OF STORMWATER.
2. COVER, CONTAINMENT, AND PROTECTION FROM VANDALISM SHALL BE PROVIDED FOR ALL CHEMICALS, LIQUID PRODUCTS, PETROLEUM PRODUCTS, AND NON–INERT WASTES PRESENT ON THE SITE (SEE CHAPTER 173–304 WAC FOR THE DEFINITION OF INERT WASTE). ONSITE FUELING TANKS SHALL INCLUDE SECONDARY CONTAINMENT.
3. MAINTENANCE AND REPAIR OF HEAVY EQUIPMENT AND VEHICLES INVOLVING OIL CHANGES, HYDRAULIC SYSTEM DRAIN DOWN, SOLVENT AND DE–GREASING CLEANING OPERATIONS, FUEL TANK DRAIN DOWN AND REMOVAL, AND OTHER ACTIVITIES WHICH MAY RESULT IN DISCHARGE OR SPILLAGE OF POLLUTANTS TO THE GROUND OR INTO STORMWATER RUNOFF MUST BE CONDUCTED USING SPILL PREVENTION MEASURES, SUCH AS DRIP PANS. CONTAMINATED SURFACES SHALL BE CLEANED IMMEDIATELY FOLLOWING ANY DISCHARGE OR SPILL INCIDENT. EMERGENCY REPAIRS MAY BE PERFORMED ONSITE USING TEMPORARY PLASTIC PLACED BENEATH AND, IF RAINING, OVER THE VEHICLE.
4. APPLICATION OF AGRICULTURAL CHEMICALS, INCLUDING FERTILIZERS AND PESTICIDES, SHALL BE CONDUCTED IN A MANNER AND AT APPLICATION RATES THAT WILL NOT RESULT IN LOSS OF CHEMICAL TO STORMWATER RUNOFF. MANUFACTURERS' RECOMMENDATIONS FOR APPLICATION RATES AND PROCEDURES SHALL BE FOLLOWED.
5. MEASURES SHALL BE USED TO PREVENT OR TREAT CONTAMINATION OF STORMWATER RUNOFF BY PH MODIFYING SOURCES. THESE SOURCES INCLUDE, BUT ARE NOT LIMITED TO, BULK CEMENT, CEMENT KILN DUST, FLY ASH, NEW CONCRETE WASHING AND CURING WATERS, WASTE STREAMS GENERATED FROM CONCRETE GRINDING AND SAWING, EXPOSED AGGREGATE PROCESSES, AND CONCRETE PUMPING AND MIXER WASHOUT WATERS. STORMWATER DISCHARGES SHALL NOT CAUSE OR CONTRIBUTE TO A VIOLATION OF THE WATER QUALITY STANDARD FOR PH IN THE RECEIVING WATER.

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

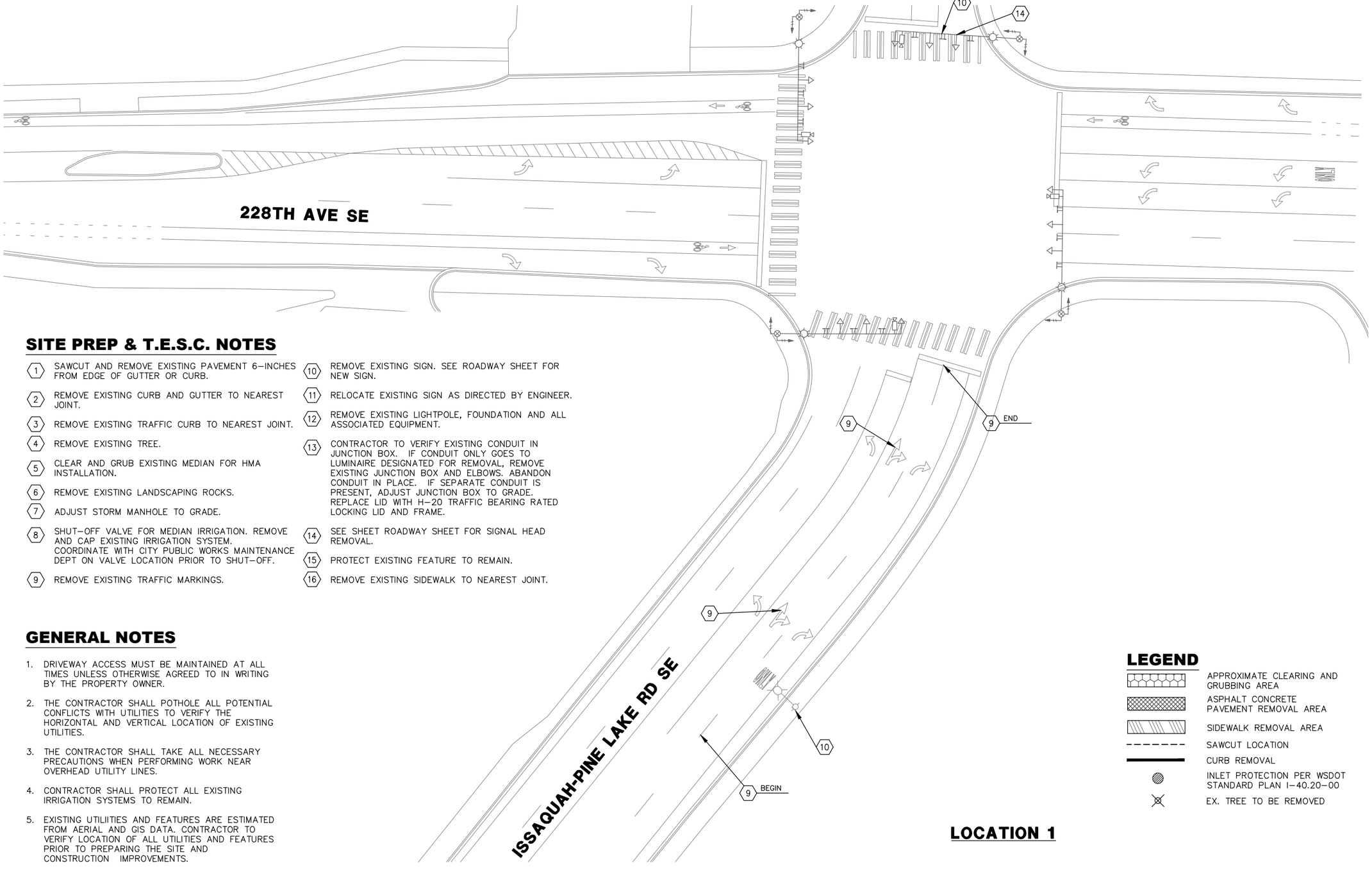
NATHAN K. ROAD — STATE OF WASHINGTON — 25813 — REGISTERED PROFESSIONAL ENGINEER — 6/24/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

STANDARD NOTES

SN02

SEC. 9, T. 24 N, R. 6 E., W.M.
SEC. 10, T. 24 N, R. 6 E., W.M.



SITE PREP & T.E.S.C. NOTES

1. SAWCUT AND REMOVE EXISTING PAVEMENT 6–INCHES FROM EDGE OF GUTTER OR CURB.
2. REMOVE EXISTING CURB AND GUTTER TO NEAREST JOINT.
3. REMOVE EXISTING TRAFFIC CURB TO NEAREST JOINT.
4. REMOVE EXISTING TREE.
5. CLEAR AND GRUB EXISTING MEDIAN FOR HMA INSTALLATION.
6. REMOVE EXISTING LANDSCAPING ROCKS.
7. ADJUST STORM MANHOLE TO GRADE.
8. SHUT–OFF VALVE FOR MEDIAN IRRIGATION. REMOVE AND CAP EXISTING IRRIGATION SYSTEM. COORDINATE WITH CITY PUBLIC WORKS MAINTENANCE DEPT ON VALVE LOCATION PRIOR TO SHUT–OFF.
9. REMOVE EXISTING TRAFFIC MARKINGS.
10. REMOVE EXISTING SIGN. SEE ROADWAY SHEET FOR NEW SIGN.
11. RELOCATE EXISTING SIGN AS DIRECTED BY ENGINEER.
12. REMOVE EXISTING LIGHTPOLE, FOUNDATION AND ALL ASSOCIATED EQUIPMENT.
13. CONTRACTOR TO VERIFY EXISTING CONDUIT IN JUNCTION BOX. IF CONDUIT ONLY GOES TO LUMINAIRE DESIGNATED FOR REMOVAL, REMOVE EXISTING JUNCTION BOX AND ELBOWS. ABANDON CONDUIT IN PLACE. IF SEPARATE CONDUIT IS PRESENT, ADJUST JUNCTION BOX TO GRADE. REPLACE LID WITH H–20 TRAFFIC BEARING RATED LOCKING LID AND FRAME.
14. SEE SHEET ROADWAY SHEET FOR SIGNAL HEAD REMOVAL.
15. PROTECT EXISTING FEATURE TO REMAIN.
16. REMOVE EXISTING SIDEWALK TO NEAREST JOINT.

GENERAL NOTES

1. DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE AGREED TO IN WRITING BY THE PROPERTY OWNER.
2. THE CONTRACTOR SHALL POTHOLE ALL POTENTIAL CONFLICTS WITH UTILITIES TO VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING UTILITIES.
3. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS WHEN PERFORMING WORK NEAR OVERHEAD UTILITY LINES.
4. CONTRACTOR SHALL PROTECT ALL EXISTING IRRIGATION SYSTEMS TO REMAIN.
5. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO PREPARING THE SITE AND CONSTRUCTION IMPROVEMENTS.

LEGEND

- APPROXIMATE CLEARING AND GRUBBING AREA
- ASPHALT CONCRETE PAVEMENT REMOVAL AREA
- SIDEWALK REMOVAL AREA
- SAWCUT LOCATION
- CURB REMOVAL
- INLET PROTECTION PER WSDOT STANDARD PLAN I–40.20–00
- EX. TREE TO BE REMOVED

20 10 0 20 40
SCALE IN FEET

LOCATION 1

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. MONKE STATE OF WASHINGTON 22815 REGISTERED PROFESSIONAL ENGINEER 6/24/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

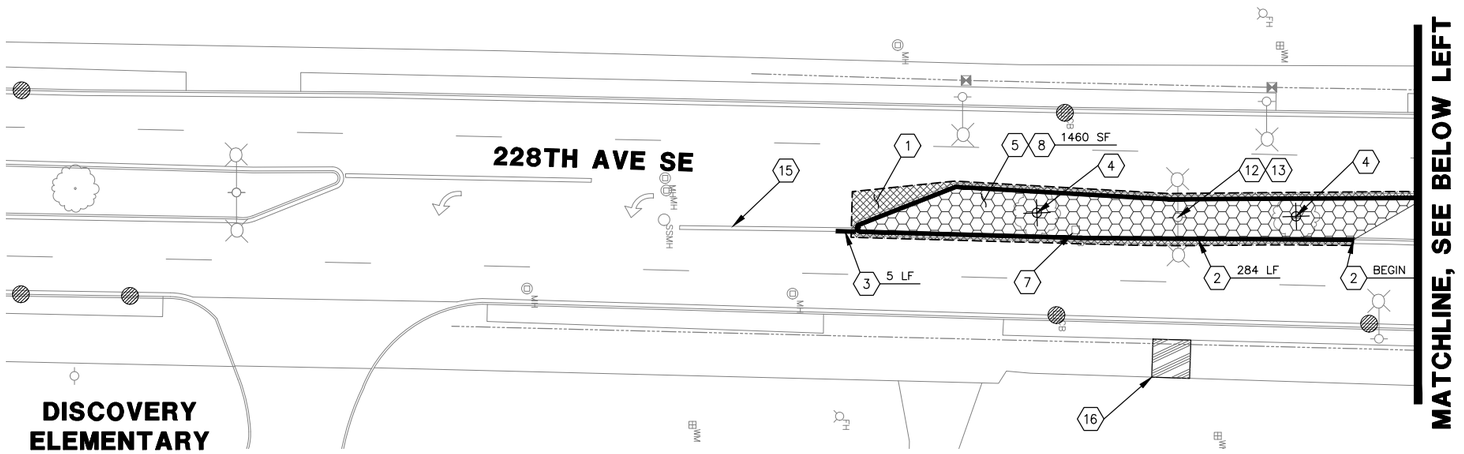
SITE PREP & T.E.S.C. PLAN

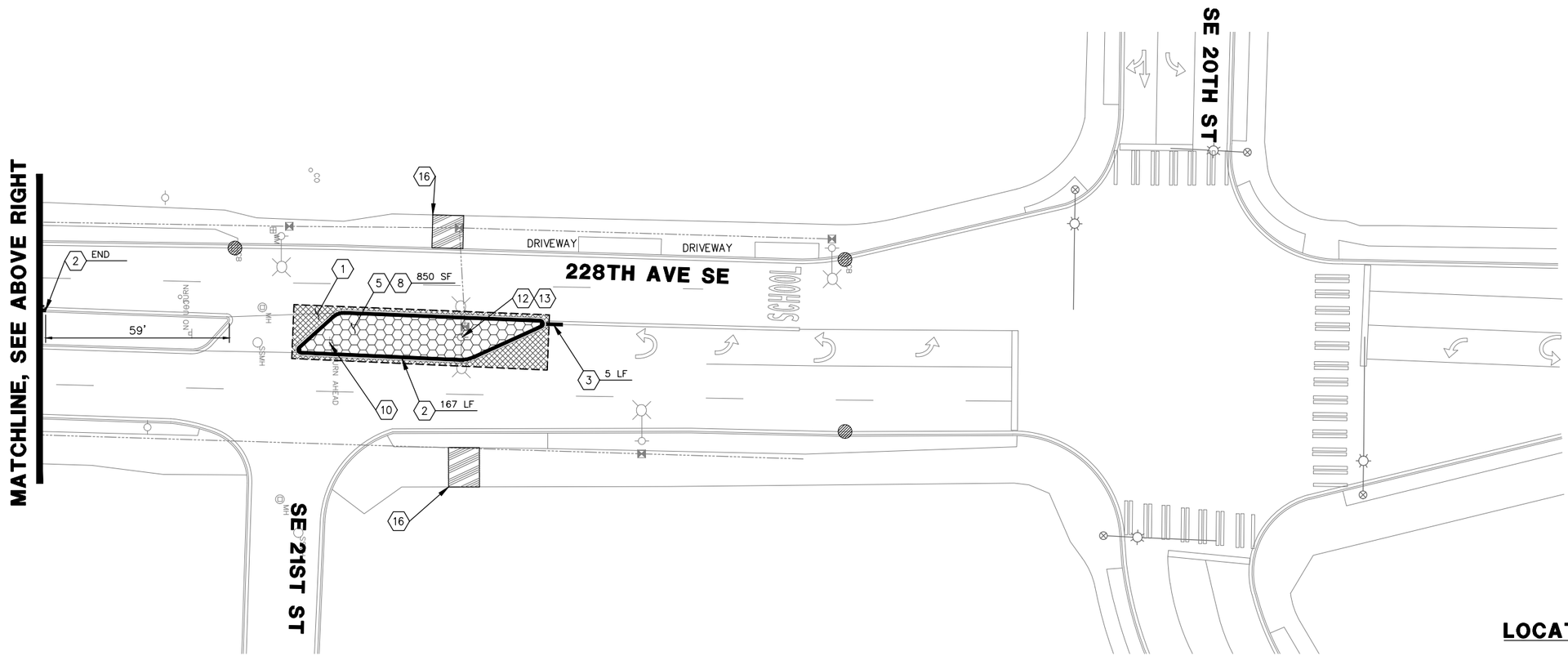
SP01

4 OF 17

By: RAKO Date: 2: 29 PM File: P: \C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTSP001COSA0023_INT.dwg
Layout: SP01

SEC. 3, T. 24 N, R. 6 E., W.M.
SEC. 4, T. 24 N, R. 6 E., W.M.





SITE PREP & T.E.S.C. NOTES

1. SAWCUT AND REMOVE EXISTING PAVEMENT 6–INCHES FROM EDGE OF GUTTER OR CURB.
2. REMOVE EXISTING CURB AND GUTTER TO NEAREST JOINT.
3. REMOVE EXISTING TRAFFIC CURB TO NEAREST JOINT.
4. REMOVE EXISTING TREE.
5. CLEAR AND GRUB EXISTING MEDIAN FOR HMA INSTALLATION.
6. REMOVE EXISTING LANDSCAPING ROCKS.
7. ADJUST STORM MANHOLE TO GRADE.
8. SHUT–OFF VALVE FOR MEDIAN IRRIGATION. REMOVE AND CAP EXISTING IRRIGATION SYSTEM. COORDINATE WITH CITY PUBLIC WORKS MAINTENANCE DEPT ON VALVE LOCATION PRIOR TO SHUT–OFF.
9. REMOVE EXISTING TRAFFIC MARKINGS.
10. REMOVE EXISTING SIGN. SEE ROADWAY SHEET FOR NEW SIGN.
11. RELOCATE EXISTING SIGN AS DIRECTED BY ENGINEER.
12. REMOVE EXISTING LIGHTPOLE, FOUNDATION AND ALL ASSOCIATED EQUIPMENT.
13. CONTRACTOR TO VERIFY EXISTING CONDUIT IN JUNCTION BOX. IF CONDUIT ONLY GOES TO LUMINAIRE DESIGNATED FOR REMOVAL, REMOVE EXISTING JUNCTION BOX AND ELBOWS. ABANDON CONDUIT IN PLACE. IF SEPARATE CONDUIT IS PRESENT, ADJUST JUNCTION BOX TO GRADE. REPLACE LID WITH H–20 TRAFFIC BEARING RATED LOCKING LID AND FRAME.
14. SEE SHEET ROADWAY SHEET FOR SIGNAL HEAD REMOVAL.
15. PROTECT EXISTING FEATURE TO REMAIN.
16. REMOVE EXISTING SIDEWALK TO NEAREST JOINT.

GENERAL NOTES

1. DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE AGREED TO IN WRITING BY THE PROPERTY OWNER.
2. THE CONTRACTOR SHALL POTHOLE ALL POTENTIAL CONFLICTS WITH UTILITIES TO VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING UTILITIES.
3. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS WHEN PERFORMING WORK NEAR OVERHEAD UTILITY LINES.
4. CONTRACTOR SHALL PROTECT ALL EXISTING IRRIGATION SYSTEMS TO REMAIN.
5. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO PREPARING THE SITE AND CONSTRUCTION IMPROVEMENTS.

LEGEND

- APPROXIMATE CLEARING AND GRUBBING AREA
- ASPHALT CONCRETE PAVEMENT REMOVAL AREA
- SIDEWALK REMOVAL AREA
- SAWCUT LOCATION
- CURB REMOVAL
- INLET PROTECTION PER WSDOT STANDARD PLAN I–40.20–00
- EX. TREE TO BE REMOVED

LOCATION 2

20 10 0 20 40
SCALE IN FEET

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. MONKS STATE OF WASHINGTON 22481 REGISTERED PROFESSIONAL ENGINEER 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

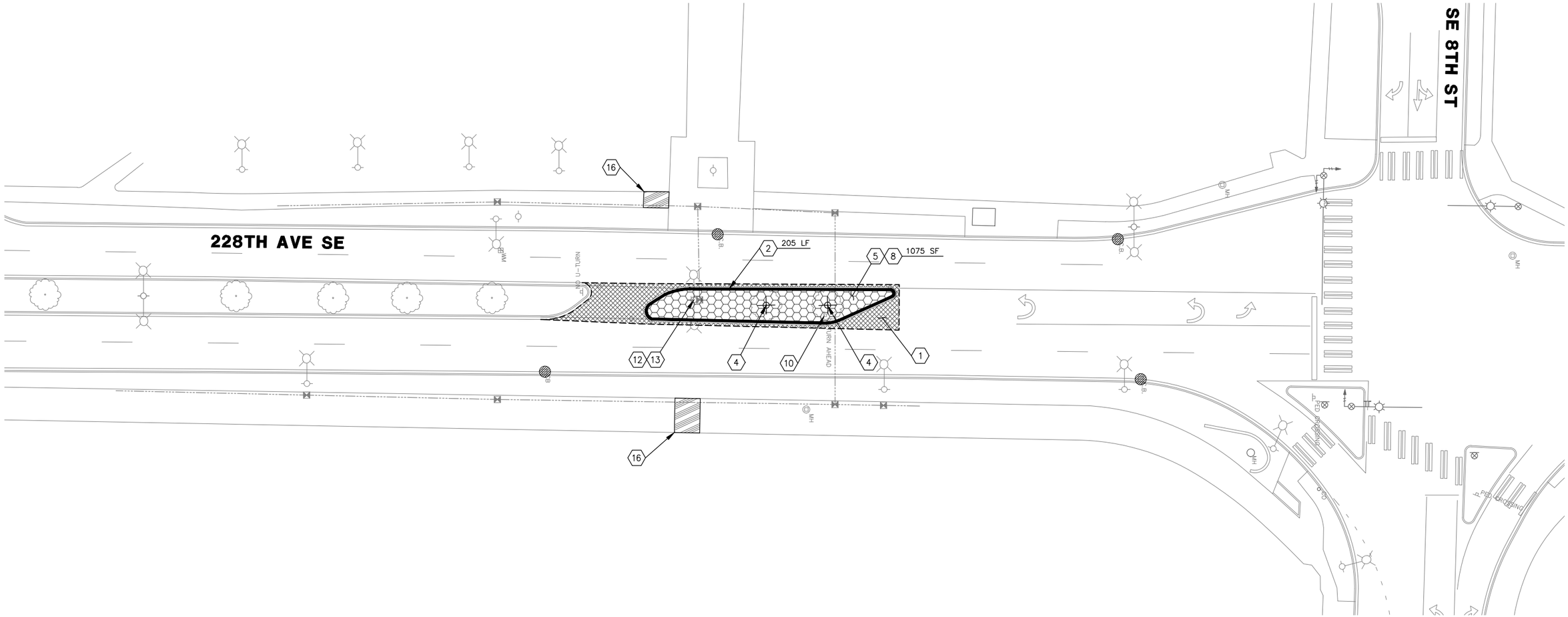
SITE PREP & T.E.S.C. PLAN

SP02

5 OF 17

By: RAKO Date: 2: 29 PM File: P: \C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTSP001COSA0023_INT.dwg
Layout: SP02

SEC. 3, T. 24 N, R. 6 E., W.M.
SEC. 4, T. 24 N, R. 6 E., W.M.



SITE PREP & T.E.S.C. NOTES

1. SAWCUT AND REMOVE EXISTING PAVEMENT 6–INCHES FROM EDGE OF GUTTER OR CURB.
2. REMOVE EXISTING CURB AND GUTTER TO NEAREST JOINT.
3. REMOVE EXISTING TRAFFIC CURB TO NEAREST JOINT.
4. REMOVE EXISTING TREE.
5. CLEAR AND GRUB EXISTING MEDIAN FOR HMA INSTALLATION.
6. REMOVE EXISTING LANDSCAPING ROCKS.
7. ADJUST STORM MANHOLE TO GRADE.
8. SHUT–OFF VALVE FOR MEDIAN IRRIGATION. REMOVE AND CAP EXISTING IRRIGATION SYSTEM. COORDINATE WITH CITY PUBLIC WORKS MAINTENANCE DEPT ON VALVE LOCATION PRIOR TO SHUT–OFF.
9. REMOVE EXISTING TRAFFIC MARKINGS.
10. REMOVE EXISTING SIGN. SEE ROADWAY SHEET FOR NEW SIGN.
11. RELOCATE EXISTING SIGN AS DIRECTED BY ENGINEER.
12. REMOVE EXISTING LIGHTPOLE, FOUNDATION AND ALL ASSOCIATED EQUIPMENT.
13. CONTRACTOR TO VERIFY EXISTING CONDUIT IN JUNCTION BOX. IF CONDUIT ONLY GOES TO LUMINAIRE DESIGNATED FOR REMOVAL, REMOVE EXISTING JUNCTION BOX AND ELBOWS. ABANDON CONDUIT IN PLACE. IF SEPARATE CONDUIT IS PRESENT, ADJUST JUNCTION BOX TO GRADE. REPLACE LID WITH H–20 TRAFFIC BEARING RATED LOCKING LID AND FRAME.
14. SEE SHEET ROADWAY SHEET FOR SIGNAL HEAD REMOVAL.
15. PROTECT EXISTING FEATURE TO REMAIN.
16. REMOVE EXISTING SIDEWALK TO NEAREST JOINT.

GENERAL NOTES

1. DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE AGREED TO IN WRITING BY THE PROPERTY OWNER.
2. THE CONTRACTOR SHALL POTHOLE ALL POTENTIAL CONFLICTS WITH UTILITIES TO VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING UTILITIES.
3. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS WHEN PERFORMING WORK NEAR OVERHEAD UTILITY LINES.
4. CONTRACTOR SHALL PROTECT ALL EXISTING IRRIGATION SYSTEMS TO REMAIN.
5. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO PREPARING THE SITE AND CONSTRUCTION IMPROVEMENTS.

LEGEND

- APPROXIMATE CLEARING AND GRUBBING AREA
- ASPHALT CONCRETE PAVEMENT REMOVAL AREA
- SIDEWALK REMOVAL AREA
- SAWCUT LOCATION
- CURB REMOVAL
- INLET PROTECTION PER WSDOT STANDARD PLAN I–40.20–00
- EX. TREE TO BE REMOVED

LOCATION 3

20 10 0 20 40
SCALE IN FEET

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. WONG — STATE OF WASHINGTON — 19913 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

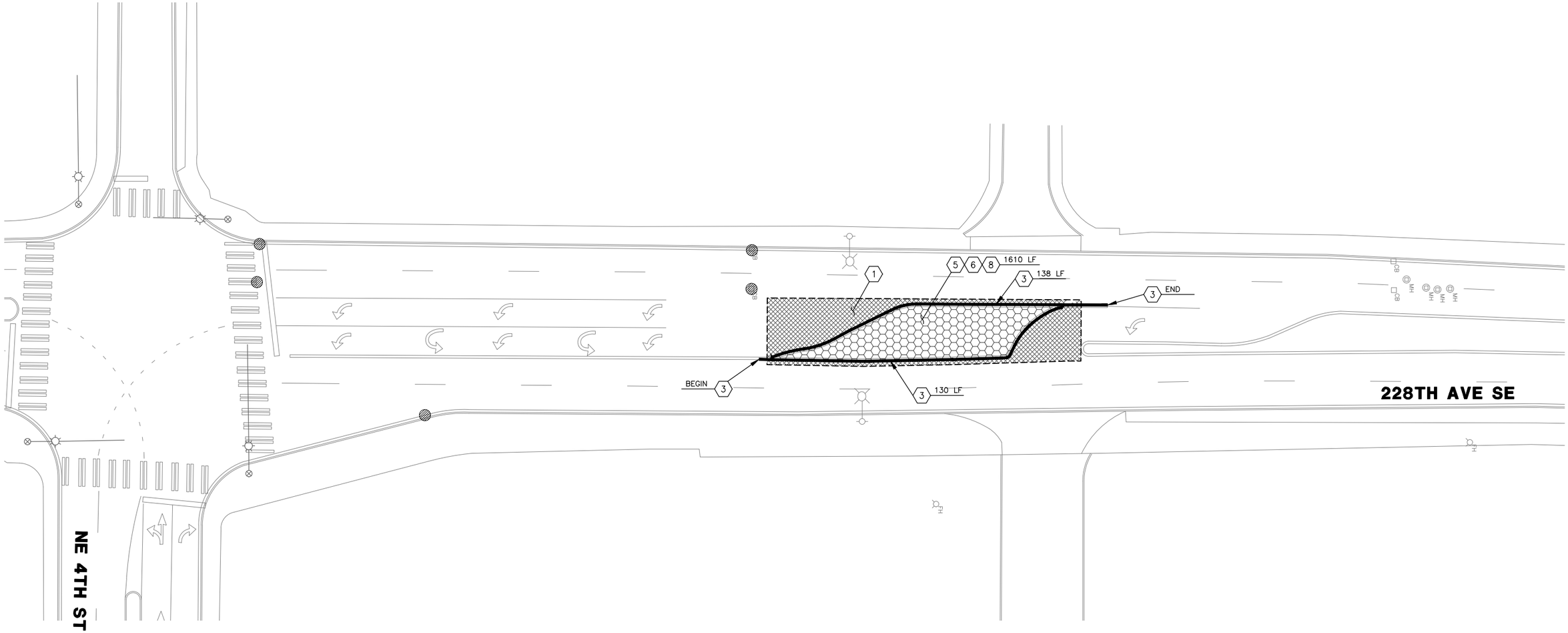
SITE PREP & T.E.S.C. PLAN

SP03

6 OF 17

By: RAKO Date: 2:29 PM File:P: \C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTSP001COSA0023_INT.dwg Layout:SP03

SEC. 33, T. 25 N, R. 6 E., W.M.
SEC. 34, T. 25 N, R. 6 E., W.M.



SITE PREP & T.E.S.C. NOTES

1. SAWCUT AND REMOVE EXISTING PAVEMENT 6–INCHES FROM EDGE OF GUTTER OR CURB.
2. REMOVE EXISTING CURB AND GUTTER TO NEAREST JOINT.
3. REMOVE EXISTING TRAFFIC CURB TO NEAREST JOINT.
4. REMOVE EXISTING TREE.
5. CLEAR AND GRUB EXISTING MEDIAN FOR HMA INSTALLATION.
6. REMOVE EXISTING LANDSCAPING ROCKS.
7. ADJUST STORM MANHOLE TO GRADE.
8. SHUT–OFF VALVE FOR MEDIAN IRRIGATION. REMOVE AND CAP EXISTING IRRIGATION SYSTEM. COORDINATE WITH CITY PUBLIC WORKS MAINTENANCE DEPT ON VALVE LOCATION PRIOR TO SHUT–OFF.
9. REMOVE EXISTING TRAFFIC MARKINGS.
10. REMOVE EXISTING SIGN. SEE ROADWAY SHEET FOR NEW SIGN.
11. RELOCATE EXISTING SIGN AS DIRECTED BY ENGINEER.
12. REMOVE EXISTING LIGHTPOLE, FOUNDATION AND ALL ASSOCIATED EQUIPMENT.
13. CONTRACTOR TO VERIFY EXISTING CONDUIT IN JUNCTION BOX. IF CONDUIT ONLY GOES TO LUMINAIRE DESIGNATED FOR REMOVAL, REMOVE EXISTING JUNCTION BOX AND ELBOWS. ABANDON CONDUIT IN PLACE. IF SEPARATE CONDUIT IS PRESENT, ADJUST JUNCTION BOX TO GRADE. REPLACE LID WITH H–20 TRAFFIC BEARING RATED LOCKING LID AND FRAME.
14. SEE SHEET ROADWAY SHEET FOR SIGNAL HEAD REMOVAL.
15. PROTECT EXISTING FEATURE TO REMAIN.
16. REMOVE EXISTING SIDEWALK TO NEAREST JOINT.

GENERAL NOTES

1. DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE AGREED TO IN WRITING BY THE PROPERTY OWNER.
2. THE CONTRACTOR SHALL POTHOLE ALL POTENTIAL CONFLICTS WITH UTILITIES TO VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING UTILITIES.
3. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS WHEN PERFORMING WORK NEAR OVERHEAD UTILITY LINES.
4. CONTRACTOR SHALL PROTECT ALL EXISTING IRRIGATION SYSTEMS TO REMAIN.
5. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO PREPARING THE SITE AND CONSTRUCTION IMPROVEMENTS.

LEGEND

- APPROXIMATE CLEARING AND GRUBBING AREA
- ASPHALT CONCRETE PAVEMENT REMOVAL AREA
- SIDEWALK REMOVAL AREA
- SAWCUT LOCATION
- CURB REMOVAL
- INLET PROTECTION PER WSDOT STANDARD PLAN I–40.20–00
- EX. TREE TO BE REMOVED

LOCATION 4

20 10 0 20 40
SCALE IN FEET

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

RENEE A. KOESTER — STATE OF WASHINGTON — 22615 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

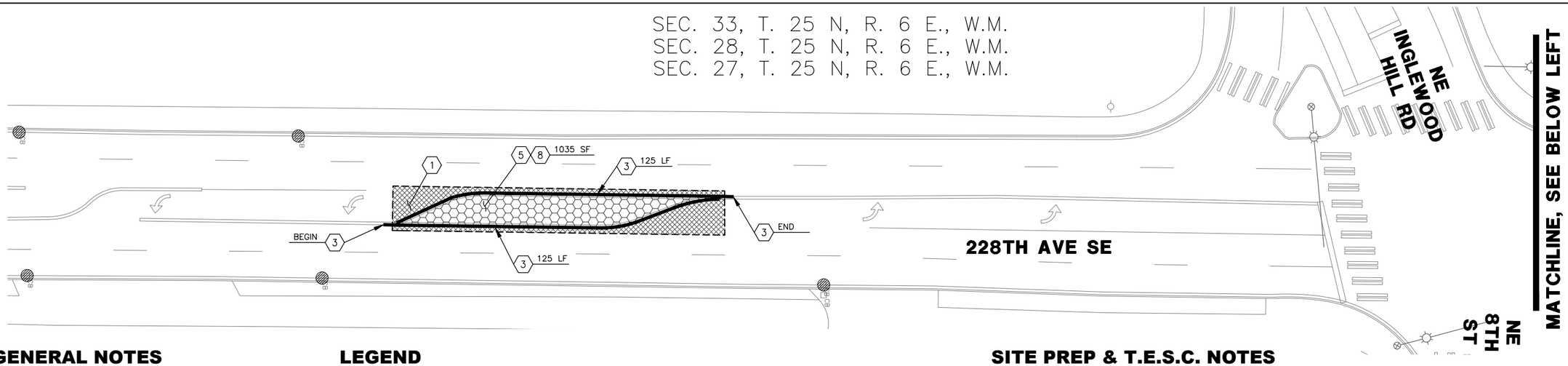
SITE PREP & T.E.S.C. PLAN

SP04

7 OF 17

By: RAKO Date: 2:29 PM File:P: \C\COSA0000023\04000CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTSP001COSA0023_INT.dwg
Layout:SP04

SEC. 33, T. 25 N, R. 6 E., W.M.
SEC. 28, T. 25 N, R. 6 E., W.M.
SEC. 27, T. 25 N, R. 6 E., W.M.



GENERAL NOTES

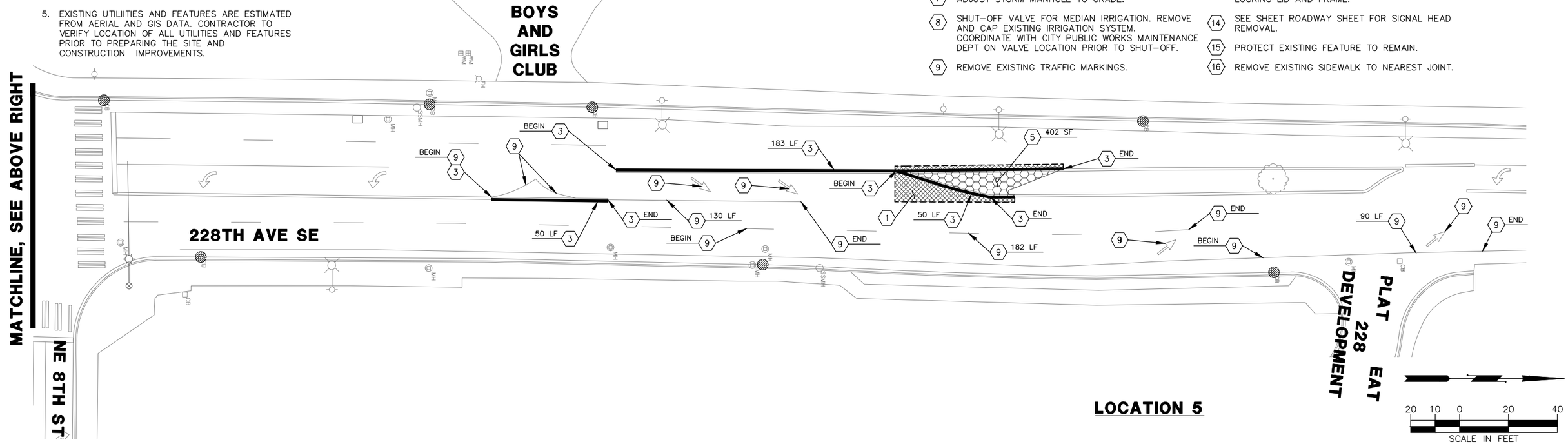
1. DRIVEWAY ACCESS MUST BE MAINTAINED AT ALL TIMES UNLESS OTHERWISE AGREED TO IN WRITING BY THE PROPERTY OWNER.
2. THE CONTRACTOR SHALL POTHOLE ALL POTENTIAL CONFLICTS WITH UTILITIES TO VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF EXISTING UTILITIES.
3. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS WHEN PERFORMING WORK NEAR OVERHEAD UTILITY LINES.
4. CONTRACTOR SHALL PROTECT ALL EXISTING IRRIGATION SYSTEMS TO REMAIN.
5. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO PREPARING THE SITE AND CONSTRUCTION IMPROVEMENTS.

LEGEND

- APPROXIMATE CLEARING AND GRUBBING AREA
- ASPHALT CONCRETE PAVEMENT REMOVAL AREA
- SIDEWALK REMOVAL AREA
- SAWCUT LOCATION
- CURB REMOVAL
- INLET PROTECTION PER WSDOT STANDARD PLAN I-40.20-00
- EX. TREE TO BE REMOVED

SITE PREP & T.E.S.C. NOTES

1. SAWCUT AND REMOVE EXISTING PAVEMENT 6-INCHES FROM EDGE OF GUTTER OR CURB.
2. REMOVE EXISTING CURB AND GUTTER TO NEAREST JOINT.
3. REMOVE EXISTING TRAFFIC CURB TO NEAREST JOINT.
4. REMOVE EXISTING TREE.
5. CLEAR AND GRUB EXISTING MEDIAN FOR HMA INSTALLATION.
6. REMOVE EXISTING LANDSCAPING ROCKS.
7. ADJUST STORM MANHOLE TO GRADE.
8. SHUT-OFF VALVE FOR MEDIAN IRRIGATION. REMOVE AND CAP EXISTING IRRIGATION SYSTEM. COORDINATE WITH CITY PUBLIC WORKS MAINTENANCE DEPT ON VALVE LOCATION PRIOR TO SHUT-OFF.
9. REMOVE EXISTING TRAFFIC MARKINGS.
10. REMOVE EXISTING SIGN. SEE ROADWAY SHEET FOR NEW SIGN.
11. RELOCATE EXISTING SIGN AS DIRECTED BY ENGINEER.
12. REMOVE EXISTING LIGHTPOLE, FOUNDATION AND ALL ASSOCIATED EQUIPMENT.
13. CONTRACTOR TO VERIFY EXISTING CONDUIT IN JUNCTION BOX. IF CONDUIT ONLY GOES TO LUMINAIRE DESIGNATED FOR REMOVAL, REMOVE EXISTING JUNCTION BOX AND ELBOWS. ABANDON CONDUIT IN PLACE. IF SEPARATE CONDUIT IS PRESENT, ADJUST JUNCTION BOX TO GRADE. REPLACE LID WITH H-20 TRAFFIC BEARING RATED LOCKING LID AND FRAME.
14. SEE SHEET ROADWAY SHEET FOR SIGNAL HEAD REMOVAL.
15. PROTECT EXISTING FEATURE TO REMAIN.
16. REMOVE EXISTING SIDEWALK TO NEAREST JOINT.



LOCATION 5

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

RENEE A. KOREMAN — STATE OF WASHINGTON — 35584 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

SP05

SITE PREP & T.E.S.C. PLAN

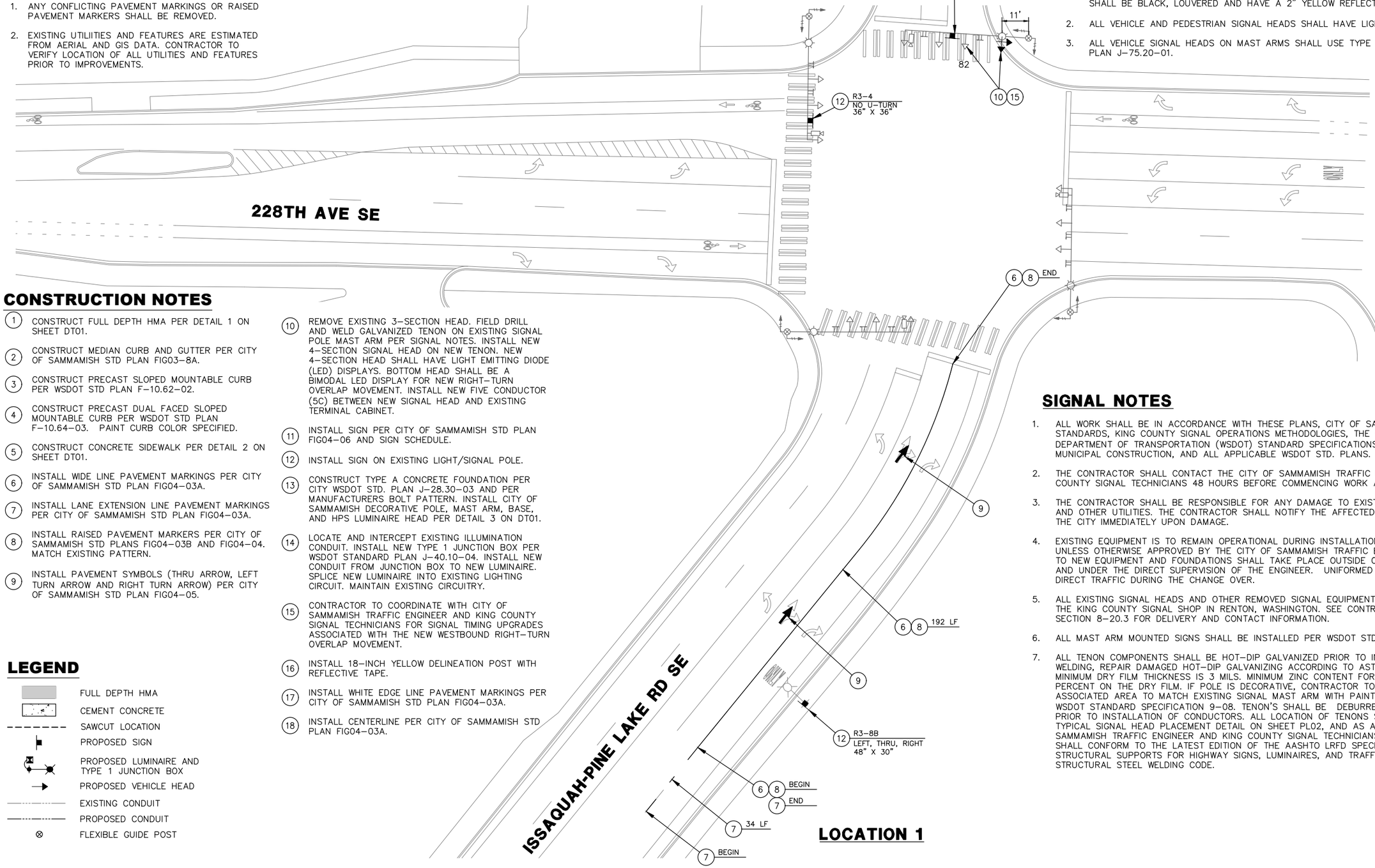
8 OF 17

By: RAKO Date: 2:29 PM File: P:\C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTSP001COSA0023_INT.dwg
Layout: SP05

GENERAL NOTES

1. ANY CONFLICTING PAVEMENT MARKINGS OR RAISED PAVEMENT MARKERS SHALL BE REMOVED.
2. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO IMPROVEMENTS.

SEC. 9, T. 24 N, R. 6 E., W.M.
SEC. 10, T. 24 N, R. 6 E., W.M.

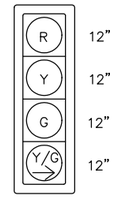


LOCATION 1

SIGNAL DISPLAY NOTES

1. ALL VEHICLE SIGNAL HEADS SHALL HAVE BACKPLATES AND TUNNEL VISORS. ALL BACKPLATES SHALL BE BLACK, LOUVERED AND HAVE A 2" YELLOW REFLECTIVE BAND OF TAPE.
2. ALL VEHICLE AND PEDESTRIAN SIGNAL HEADS SHALL HAVE LIGHT EMITTING DIODES (LED).
3. ALL VEHICLE SIGNAL HEADS ON MAST ARMS SHALL USE TYPE M MOUNTING PER WSDOT STD. PLAN J-75.20-01.

SIGNAL DISPLAY



VEHICLE HEAD NO. 82

CONSTRUCTION NOTES

1. CONSTRUCT FULL DEPTH HMA PER DETAIL 1 ON SHEET DT01.
2. CONSTRUCT MEDIAN CURB AND GUTTER PER CITY OF SAMMAMISH STD PLAN FIG03-8A.
3. CONSTRUCT PRECAST SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F-10.62-02.
4. CONSTRUCT PRECAST DUAL FACED SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F-10.64-03. PAINT CURB COLOR SPECIFIED.
5. CONSTRUCT CONCRETE SIDEWALK PER DETAIL 2 ON SHEET DT01.
6. INSTALL WIDE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04-03A.
7. INSTALL LANE EXTENSION LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04-03A.
8. INSTALL RAISED PAVEMENT MARKERS PER CITY OF SAMMAMISH STD PLANS FIG04-03B AND FIG04-04. MATCH EXISTING PATTERN.
9. INSTALL PAVEMENT SYMBOLS (THRU ARROW, LEFT TURN ARROW AND RIGHT TURN ARROW) PER CITY OF SAMMAMISH STD PLAN FIG04-05.
10. REMOVE EXISTING 3-SECTION HEAD. FIELD DRILL AND WELD GALVANIZED TENON ON EXISTING SIGNAL POLE MAST ARM PER SIGNAL NOTES. INSTALL NEW 4-SECTION SIGNAL HEAD ON NEW TENON. NEW 4-SECTION HEAD SHALL HAVE LIGHT EMITTING DIODE (LED) DISPLAYS. BOTTOM HEAD SHALL BE A BIMODAL LED DISPLAY FOR NEW RIGHT-TURN OVERLAP MOVEMENT. INSTALL NEW FIVE CONDUCTOR (5C) BETWEEN NEW SIGNAL HEAD AND EXISTING TERMINAL CABINET.
11. INSTALL SIGN PER CITY OF SAMMAMISH STD PLAN FIG04-06 AND SIGN SCHEDULE.
12. INSTALL SIGN ON EXISTING LIGHT/SIGNAL POLE.
13. CONSTRUCT TYPE A CONCRETE FOUNDATION PER CITY WSDOT STD. PLAN J-28.30-03 AND PER MANUFACTURERS BOLT PATTERN. INSTALL CITY OF SAMMAMISH DECORATIVE POLE, MAST ARM, BASE, AND HPS LUMINAIRE HEAD PER DETAIL 3 ON DT01.
14. LOCATE AND INTERCEPT EXISTING ILLUMINATION CONDUIT. INSTALL NEW TYPE 1 JUNCTION BOX PER WSDOT STANDARD PLAN J-40.10-04. INSTALL NEW CONDUIT FROM JUNCTION BOX TO NEW LUMINAIRE. SPLICE NEW LUMINAIRE INTO EXISTING LIGHTING CIRCUIT. MAINTAIN EXISTING CIRCUITRY.
15. CONTRACTOR TO COORDINATE WITH CITY OF SAMMAMISH TRAFFIC ENGINEER AND KING COUNTY SIGNAL TECHNICIANS FOR SIGNAL TIMING UPGRADES ASSOCIATED WITH THE NEW WESTBOUND RIGHT-TURN OVERLAP MOVEMENT.
16. INSTALL 18-INCH YELLOW DELINEATION POST WITH REFLECTIVE TAPE.
17. INSTALL WHITE EDGE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04-03A.
18. INSTALL CENTERLINE PER CITY OF SAMMAMISH STD PLAN FIG04-03A.

LEGEND

- FULL DEPTH HMA
- CEMENT CONCRETE
- SAWCUT LOCATION
- PROPOSED SIGN
- PROPOSED LUMINAIRE AND TYPE 1 JUNCTION BOX
- PROPOSED VEHICLE HEAD
- EXISTING CONDUIT
- PROPOSED CONDUIT
- FLEXIBLE GUIDE POST

SIGNAL NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THESE PLANS, CITY OF SAMMAMISH PUBLIC WORKS STANDARDS, KING COUNTY SIGNAL OPERATIONS METHODOLOGIES, THE WASHINGTON STATE DEPARTMENT OF TRANSPORTATION (WSDOT) STANDARD SPECIFICATIONS FOR ROAD, BRIDGE AND MUNICIPAL CONSTRUCTION, AND ALL APPLICABLE WSDOT STD. PLANS.
2. THE CONTRACTOR SHALL CONTACT THE CITY OF SAMMAMISH TRAFFIC ENGINEER AND KING COUNTY SIGNAL TECHNICIANS 48 HOURS BEFORE COMMENCING WORK AT ANY INTERSECTION,
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO EXISTING SIGNAL EQUIPMENT AND OTHER UTILITIES. THE CONTRACTOR SHALL NOTIFY THE AFFECTED UTILITY COMPANY AND THE CITY IMMEDIATELY UPON DAMAGE.
4. EXISTING EQUIPMENT IS TO REMAIN OPERATIONAL DURING INSTALLATION OF NEW EQUIPMENT, UNLESS OTHERWISE APPROVED BY THE CITY OF SAMMAMISH TRAFFIC ENGINEER. CHANGE OVER TO NEW EQUIPMENT AND FOUNDATIONS SHALL TAKE PLACE OUTSIDE OF PEAK TRAFFIC HOURS, AND UNDER THE DIRECT SUPERVISION OF THE ENGINEER. UNIFORMED POLICE OFFICER SHALL DIRECT TRAFFIC DURING THE CHANGE OVER.
5. ALL EXISTING SIGNAL HEADS AND OTHER REMOVED SIGNAL EQUIPMENT SHALL BE RETURNED TO THE KING COUNTY SIGNAL SHOP IN RENTON, WASHINGTON. SEE CONTRACT SPECIAL PROVISIONS SECTION 8-20.3 FOR DELIVERY AND CONTACT INFORMATION.
6. ALL MAST ARM MOUNTED SIGNS SHALL BE INSTALLED PER WSDOT STD. PLAN G-30.10-04.
7. ALL TENON COMPONENTS SHALL BE HOT-DIP GALVANIZED PRIOR TO INSTALLATION. AFTER FIELD WELDING, REPAIR DAMAGED HOT-DIP GALVANIZING ACCORDING TO ASTM A780 AND ASTM A123. MINIMUM DRY FILM THICKNESS IS 3 MILS. MINIMUM ZINC CONTENT FOR METHOD A2 IS 92 PERCENT ON THE DRY FILM. IF POLE IS DECORATIVE, CONTRACTOR TO PAINT TENON AND ASSOCIATED AREA TO MATCH EXISTING SIGNAL MAST ARM WITH PAINT IN ACCORDANCE WITH WSDOT STANDARD SPECIFICATION 9-08. TENON'S SHALL BE DEBURRED AND EDGES TREATED PRIOR TO INSTALLATION OF CONDUCTORS. ALL LOCATION OF TENONS SHOULD BE INSTALLED PER TYPICAL SIGNAL HEAD PLACEMENT DETAIL ON SHEET PL02, AND AS APPROVED BY THE CITY OF SAMMAMISH TRAFFIC ENGINEER AND KING COUNTY SIGNAL TECHNICIANS. INSTALLATION OF TENON SHALL CONFORM TO THE LATEST EDITION OF THE AASHTO LRFD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS AND AWS D1.1 STRUCTURAL STEEL WELDING CODE.

20 10 0 20 40
SCALE IN FEET

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. HOWE — STATE OF WASHINGTON — 45913 — REGISTERED PROFESSIONAL ENGINEER — 6/24/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

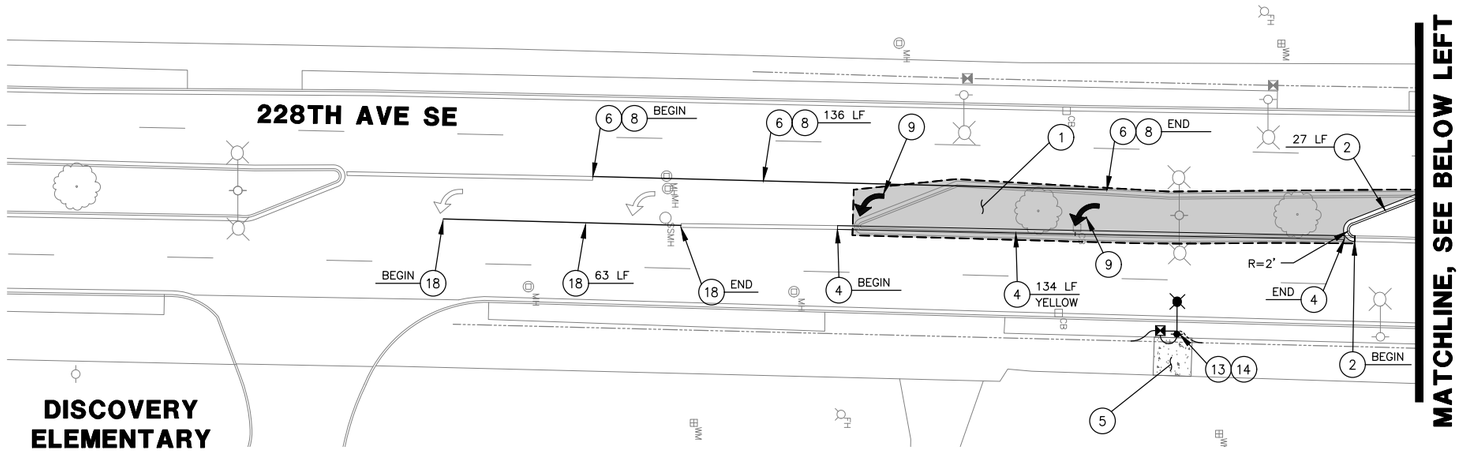
ROADWAY PLAN

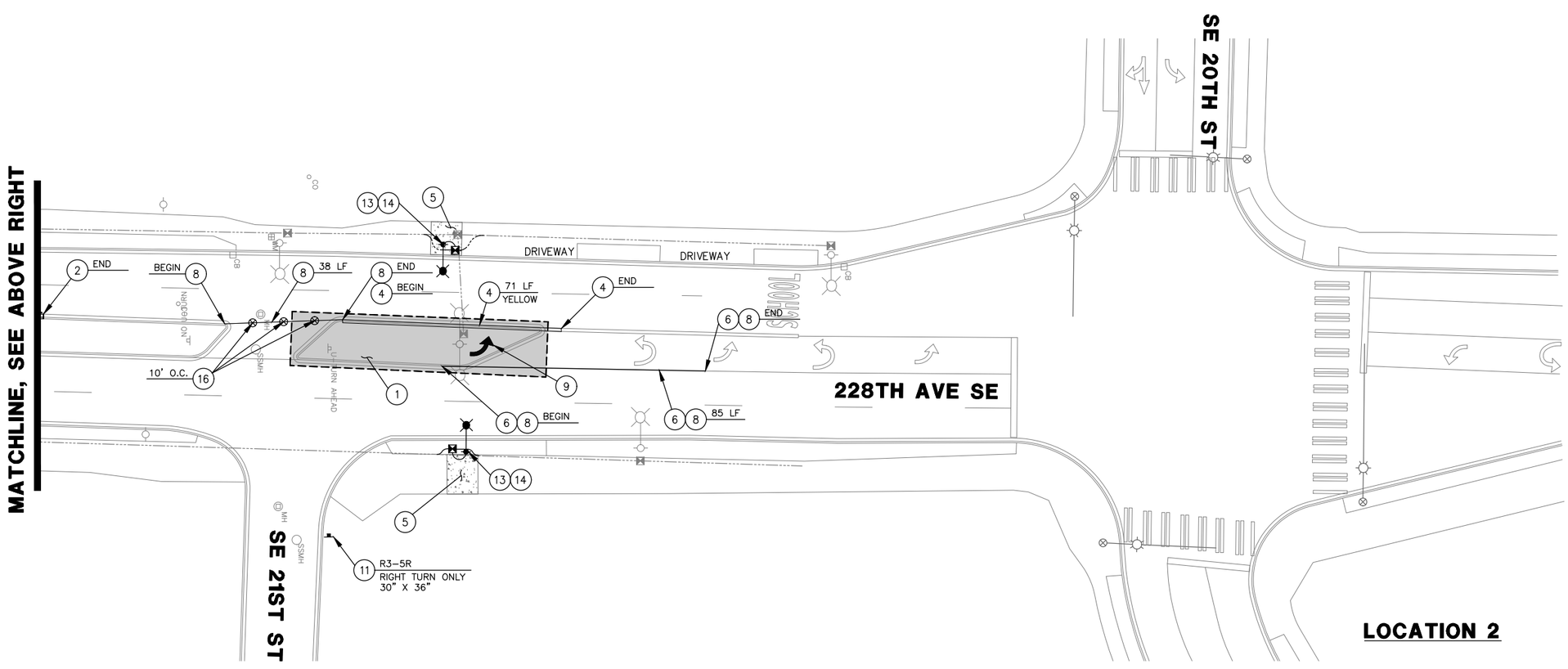
PL01

9 OF 17

By: RAKO Date: 2:29 PM File:P:\C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTRD001COSA0023_INT.dwg
Layout:PL01

SEC. 3, T. 24 N, R. 6 E., W.M.
SEC. 4, T. 24 N, R. 6 E., W.M.





CONSTRUCTION NOTES

1. CONSTRUCT FULL DEPTH HMA PER DETAIL 1 ON SHEET DT01.
2. CONSTRUCT MEDIAN CURB AND GUTTER PER CITY OF SAMMAMISH STD PLAN FIG03-8A.
3. CONSTRUCT PRECAST SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F-10.62-02.
4. CONSTRUCT PRECAST DUAL FACED SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F-10.64-03. PAINT CURB COLOR SPECIFIED.
5. CONSTRUCT CONCRETE SIDEWALK PER DETAIL 2 ON SHEET DT01.
6. INSTALL WIDE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04-03A.
7. INSTALL LANE EXTENSION LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04-03A.
8. INSTALL RAISED PAVEMENT MARKERS PER CITY OF SAMMAMISH STD PLANS FIG04-03B AND FIG04-04. MATCH EXISTING PATTERN.
9. INSTALL PAVEMENT SYMBOLS (THRU ARROW, LEFT TURN ARROW AND RIGHT TURN ARROW) PER CITY OF SAMMAMISH STD PLAN FIG04-05.
10. REMOVE EXISTING 3-SECTION HEAD. FIELD DRILL AND WELD GALVANIZED TENON ON EXISTING SIGNAL POLE MAST ARM PER SIGNAL NOTES. INSTALL NEW 4-SECTION SIGNAL HEAD ON NEW TENON. NEW 4-SECTION HEAD SHALL HAVE LIGHT EMITTING DIODE (LED) DISPLAYS. BOTTOM HEAD SHALL BE A BIMODAL LED DISPLAY FOR NEW RIGHT-TURN OVERLAP MOVEMENT. INSTALL NEW FIVE CONDUCTOR (5C) BETWEEN NEW SIGNAL HEAD AND EXISTING TERMINAL CABINET.
11. INSTALL SIGN PER CITY OF SAMMAMISH STD PLAN FIG04-06 AND SIGN SCHEDULE.
12. INSTALL SIGN ON EXISTING LIGHT/SIGNAL POLE.
13. CONSTRUCT TYPE A CONCRETE FOUNDATION PER CITY WSDOT STD. PLAN J-28.30-03 AND PER MANUFACTURERS BOLT PATTERN. INSTALL CITY OF SAMMAMISH DECORATIVE POLE, MAST ARM, BASE, AND HPS LUMINAIRE HEAD PER DETAIL 3 ON DT01.
14. LOCATE AND INTERCEPT EXISTING ILLUMINATION CONDUIT. INSTALL NEW TYPE 1 JUNCTION BOX PER WSDOT STANDARD PLAN J-40.10-04. INSTALL NEW CONDUIT FROM JUNCTION BOX TO NEW LUMINAIRE. SPLICE NEW LUMINAIRE INTO EXISTING LIGHTING CIRCUIT. MAINTAIN EXISTING CIRCUITRY.
15. CONTRACTOR TO COORDINATE WITH CITY OF SAMMAMISH TRAFFIC ENGINEER AND KING COUNTY SIGNAL TECHNICIANS FOR SIGNAL TIMING UPGRADES ASSOCIATED WITH THE NEW WESTBOUND RIGHT-TURN OVERLAP MOVEMENT.
16. INSTALL 18-INCH YELLOW DELINEATION POST WITH REFLECTIVE TAPE.
17. INSTALL WHITE EDGE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04-03A.
18. INSTALL CENTERLINE PER CITY OF SAMMAMISH STD PLAN FIG04-03A.

GENERAL NOTES

1. ANY CONFLICTING PAVEMENT MARKINGS OR RAISED PAVEMENT MARKERS SHALL BE REMOVED.
2. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO IMPROVEMENTS.

LEGEND

- FULL DEPTH HMA
- CEMENT CONCRETE
- SAWCUT LOCATION
- PROPOSED SIGN
- PROPOSED LUMINAIRE AND TYPE 1 JUNCTION BOX
- PROPOSED VEHICLE HEAD
- EXISTING CONDUIT
- PROPOSED CONDUIT
- FLEXIBLE GUIDE POST

LOCATION 2

20 10 0 20 40
SCALE IN FEET

By: RAKO Date: 2:29 PM File: P:\C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTRD001COSA0023_INT.dwg
Layout: PL02

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. RAKO — STATE OF WASHINGTON — 45813 — REGISTERED PROFESSIONAL ENGINEER — 6/24/18

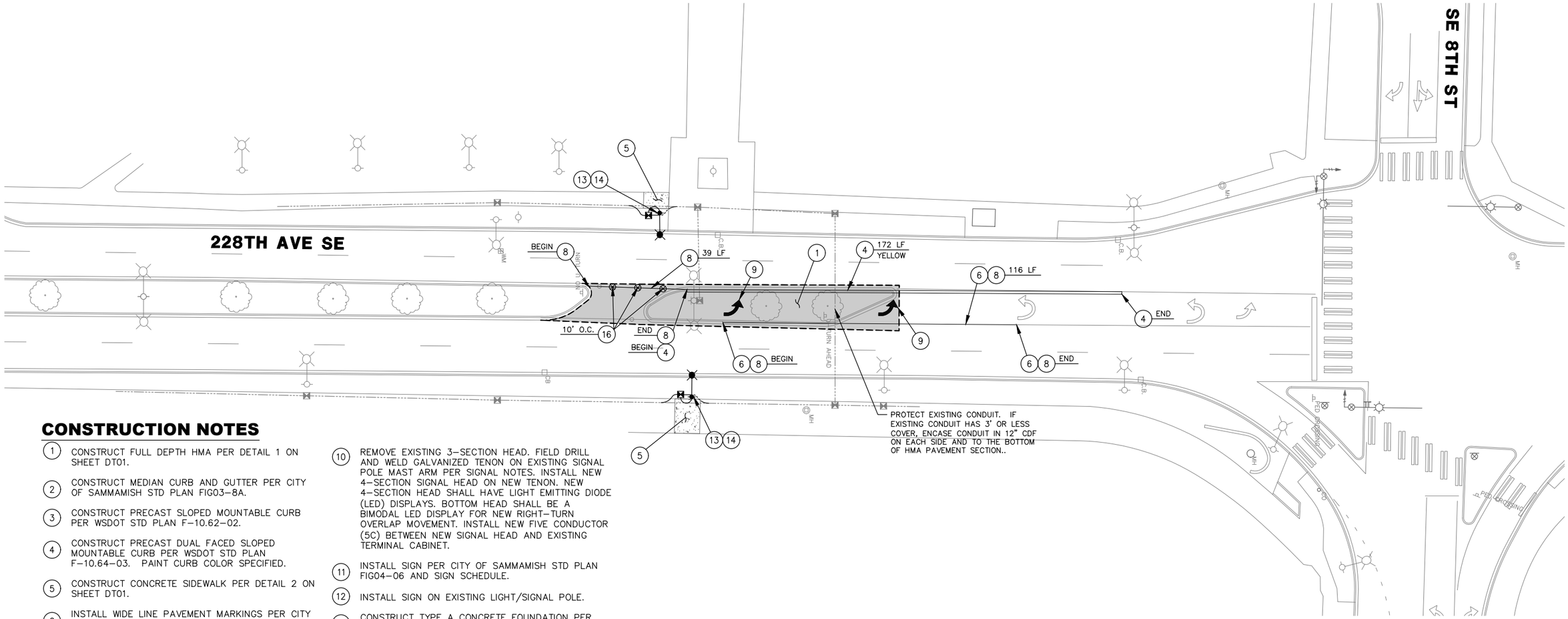
CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

ROADWAY PLAN

PL02

10 OF 17

SEC. 3, T. 24 N, R. 6 E., W.M.
SEC. 4, T. 24 N, R. 6 E., W.M.



CONSTRUCTION NOTES

1. CONSTRUCT FULL DEPTH HMA PER DETAIL 1 ON SHEET DT01.
2. CONSTRUCT MEDIAN CURB AND GUTTER PER CITY OF SAMMAMISH STD PLAN FIG03–8A.
3. CONSTRUCT PRECAST SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F–10.62–02.
4. CONSTRUCT PRECAST DUAL FACED SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F–10.64–03. PAINT CURB COLOR SPECIFIED.
5. CONSTRUCT CONCRETE SIDEWALK PER DETAIL 2 ON SHEET DT01.
6. INSTALL WIDE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
7. INSTALL LANE EXTENSION LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
8. INSTALL RAISED PAVEMENT MARKERS PER CITY OF SAMMAMISH STD PLANS FIG04–03B AND FIG04–04. MATCH EXISTING PATTERN.
9. INSTALL PAVEMENT SYMBOLS (THRU ARROW, LEFT TURN ARROW AND RIGHT TURN ARROW) PER CITY OF SAMMAMISH STD PLAN FIG04–05.
10. REMOVE EXISTING 3–SECTION HEAD. FIELD DRILL AND WELD GALVANIZED TENON ON EXISTING SIGNAL POLE MAST ARM PER SIGNAL NOTES. INSTALL NEW 4–SECTION SIGNAL HEAD ON NEW TENON. NEW 4–SECTION HEAD SHALL HAVE LIGHT EMITTING DIODE (LED) DISPLAYS. BOTTOM HEAD SHALL BE A BIMODAL LED DISPLAY FOR NEW RIGHT–TURN OVERLAP MOVEMENT. INSTALL NEW FIVE CONDUCTOR (5C) BETWEEN NEW SIGNAL HEAD AND EXISTING TERMINAL CABINET.
11. INSTALL SIGN PER CITY OF SAMMAMISH STD PLAN FIG04–06 AND SIGN SCHEDULE.
12. INSTALL SIGN ON EXISTING LIGHT/SIGNAL POLE.
13. CONSTRUCT TYPE A CONCRETE FOUNDATION PER CITY WSDOT STD. PLAN J–28.30–03 AND PER MANUFACTURERS BOLT PATTERN. INSTALL CITY OF SAMMAMISH DECORATIVE POLE, MAST ARM, BASE, AND HPS LUMINAIRE HEAD PER DETAIL 3 ON DT01.
14. LOCATE AND INTERCEPT EXISTING ILLUMINATION CONDUIT. INSTALL NEW TYPE 1 JUNCTION BOX PER WSDOT STANDARD PLAN J–40.10–04. INSTALL NEW CONDUIT FROM JUNCTION BOX TO NEW LUMINAIRE. SPLICE NEW LUMINAIRE INTO EXISTING LIGHTING CIRCUIT. MAINTAIN EXISTING CIRCUITRY.
15. CONTRACTOR TO COORDINATE WITH CITY OF SAMMAMISH TRAFFIC ENGINEER AND KING COUNTY SIGNAL TECHNICIANS FOR SIGNAL TIMING UPGRADES ASSOCIATED WITH THE NEW WESTBOUND RIGHT–TURN OVERLAP MOVEMENT.
16. INSTALL 18–INCH YELLOW DELINEATION POST WITH REFLECTIVE TAPE.
17. INSTALL WHITE EDGE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
18. INSTALL CENTERLINE PER CITY OF SAMMAMISH STD PLAN FIG04–03A.

LEGEND

- FULL DEPTH HMA
- CEMENT CONCRETE
- SAWCUT LOCATION
- PROPOSED SIGN
- PROPOSED LUMINAIRE AND TYPE 1 JUNCTION BOX
- PROPOSED VEHICLE HEAD
- EXISTING CONDUIT
- PROPOSED CONDUIT
- FLEXIBLE GUIDE POST

GENERAL NOTES

1. ANY CONFLICTING PAVEMENT MARKINGS OR RAISED PAVEMENT MARKERS SHALL BE REMOVED.
2. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO IMPROVEMENTS.

LOCATION 3

20 10 0 20 40
SCALE IN FEET

NO.	DATE	BY	APPR	REVISION

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. WONG — STATE OF WASHINGTON — 23813 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

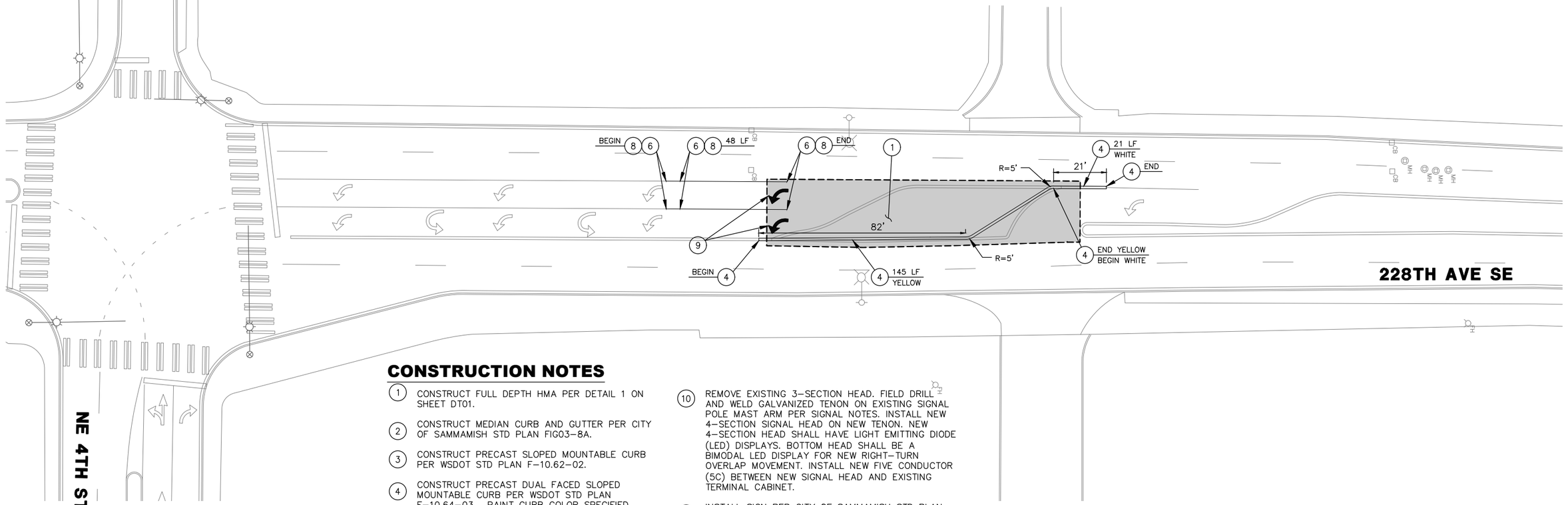
ROADWAY PLAN

PL03

11 OF 17

By: RAKO Date: 2:29 PM File:P:\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTRD001COSA0023_INT.dwg Layout:PL03

SEC. 33, T. 25 N, R. 6 E., W.M.
SEC. 34, T. 25 N, R. 6 E., W.M.



CONSTRUCTION NOTES

1. CONSTRUCT FULL DEPTH HMA PER DETAIL 1 ON SHEET DT01.
2. CONSTRUCT MEDIAN CURB AND GUTTER PER CITY OF SAMMAMISH STD PLAN FIG03–8A.
3. CONSTRUCT PRECAST SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F–10.62–02.
4. CONSTRUCT PRECAST DUAL FACED SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F–10.64–03. PAINT CURB COLOR SPECIFIED.
5. CONSTRUCT CONCRETE SIDEWALK PER DETAIL 2 ON SHEET DT01.
6. INSTALL WIDE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
7. INSTALL LANE EXTENSION LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
8. INSTALL RAISED PAVEMENT MARKERS PER CITY OF SAMMAMISH STD PLANS FIG04–03B AND FIG04–04. MATCH EXISTING PATTERN.
9. INSTALL PAVEMENT SYMBOLS (THRU ARROW, LEFT TURN ARROW AND RIGHT TURN ARROW) PER CITY OF SAMMAMISH STD PLAN FIG04–05.
10. REMOVE EXISTING 3–SECTION HEAD. FIELD DRILL AND WELD GALVANIZED TENON ON EXISTING SIGNAL POLE MAST ARM PER SIGNAL NOTES. INSTALL NEW 4–SECTION SIGNAL HEAD ON NEW TENON. NEW 4–SECTION HEAD SHALL HAVE LIGHT EMITTING DIODE (LED) DISPLAYS. BOTTOM HEAD SHALL BE A BIMODAL LED DISPLAY FOR NEW RIGHT–TURN OVERLAP MOVEMENT. INSTALL NEW FIVE CONDUCTOR (5C) BETWEEN NEW SIGNAL HEAD AND EXISTING TERMINAL CABINET.
11. INSTALL SIGN PER CITY OF SAMMAMISH STD PLAN FIG04–06 AND SIGN SCHEDULE.
12. INSTALL SIGN ON EXISTING LIGHT/SIGNAL POLE.
13. CONSTRUCT TYPE A CONCRETE FOUNDATION PER CITY WSDOT STD. PLAN J–28.30–03 AND PER MANUFACTURERS BOLT PATTERN. INSTALL CITY OF SAMMAMISH DECORATIVE POLE, MAST ARM, BASE, AND HPS LUMINAIRE HEAD PER DETAIL 3 ON DT01.
14. LOCATE AND INTERCEPT EXISTING ILLUMINATION CONDUIT. INSTALL NEW TYPE 1 JUNCTION BOX PER WSDOT STANDARD PLAN J–40.10–04. INSTALL NEW CONDUIT FROM JUNCTION BOX TO NEW LUMINAIRE. SPLICE NEW LUMINAIRE INTO EXISTING LIGHTING CIRCUIT. MAINTAIN EXISTING CIRCUITRY.
15. CONTRACTOR TO COORDINATE WITH CITY OF SAMMAMISH TRAFFIC ENGINEER AND KING COUNTY SIGNAL TECHNICIANS FOR SIGNAL TIMING UPGRADES ASSOCIATED WITH THE NEW WESTBOUND RIGHT–TURN OVERLAP MOVEMENT.
16. INSTALL 18–INCH YELLOW DELINEATION POST WITH REFLECTIVE TAPE.
17. INSTALL WHITE EDGE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
18. INSTALL CENTERLINE PER CITY OF SAMMAMISH STD PLAN FIG04–03A.

GENERAL NOTES

1. ANY CONFLICTING PAVEMENT MARKINGS OR RAISED PAVEMENT MARKERS SHALL BE REMOVED.
2. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO IMPROVEMENTS.

LEGEND

- FULL DEPTH HMA
- CEMENT CONCRETE
- SAWCUT LOCATION
- PROPOSED SIGN
- PROPOSED LUMINAIRE AND TYPE 1 JUNCTION BOX
- PROPOSED VEHICLE HEAD
- EXISTING CONDUIT
- PROPOSED CONDUIT
- FLEXIBLE GUIDE POST

20 10 0 20 40
SCALE IN FEET

LOCATION 4

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

RENEE A. KOESTER — STATE OF WASHINGTON — 35584 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

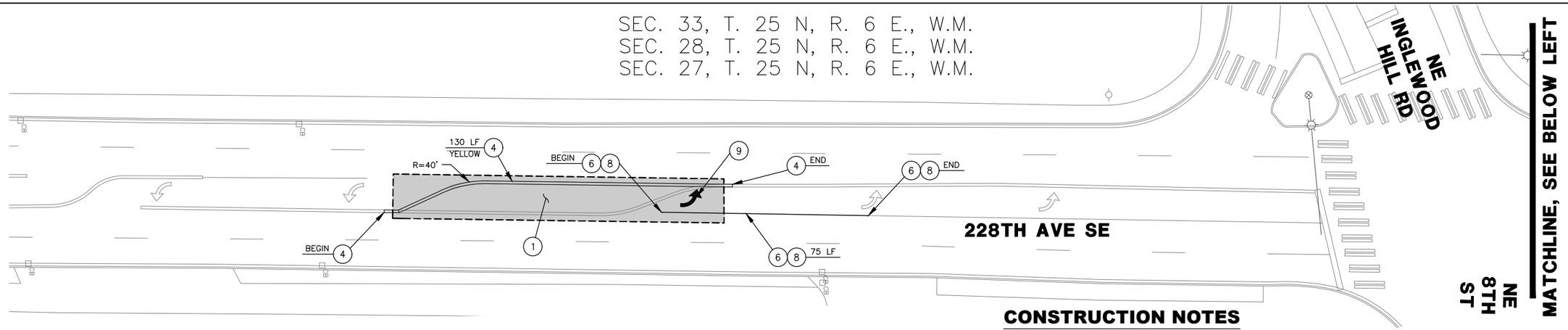
ROADWAY PLAN

PL04

12 OF 17

By: RAKO Date: 2:29 PM File:P:\C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTRD001COSA0023_INT.dwg
Layout:PL04

SEC. 33, T. 25 N, R. 6 E., W.M.
SEC. 28, T. 25 N, R. 6 E., W.M.
SEC. 27, T. 25 N, R. 6 E., W.M.



GENERAL NOTES

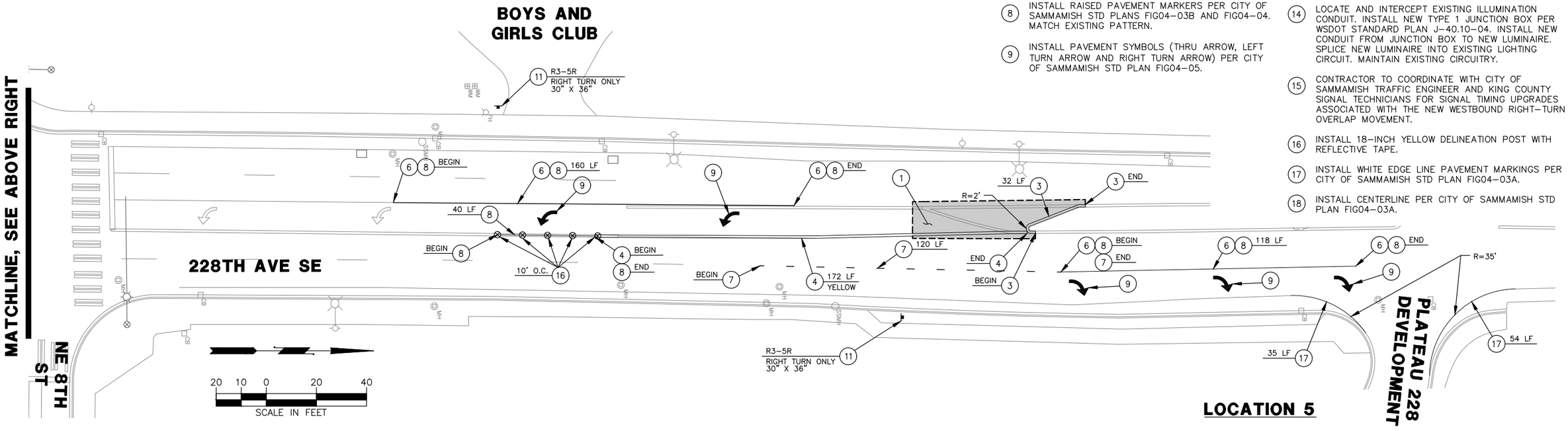
1. ANY CONFLICTING PAVEMENT MARKINGS OR RAISED PAVEMENT MARKERS SHALL BE REMOVED.
2. EXISTING UTILIITIES AND FEATURES ARE ESTIMATED FROM AERIAL AND GIS DATA. CONTRACTOR TO VERIFY LOCATION OF ALL UTILITIES AND FEATURES PRIOR TO IMPROVEMENTS.

LEGEND

- FULL DEPTH HMA
- CEMENT CONCRETE
- SAWCUT LOCATION
- PROPOSED SIGN
- PROPOSED LUMINAIRE AND TYPE 1 JUNCTION BOX
- PROPOSED VEHICLE HEAD
- EXISTING CONDUIT
- PROPOSED CONDUIT
- FLEXIBLE GUIDE POST

CONSTRUCTION NOTES

1. CONSTRUCT FULL DEPTH HMA PER DETAIL 1 ON SHEET DT01.
2. CONSTRUCT MEDIAN CURB AND GUTTER PER CITY OF SAMMAMISH STD PLAN FIG03–8A.
3. CONSTRUCT PRECAST SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F–10.62–02.
4. CONSTRUCT PRECAST DUAL FACED SLOPED MOUNTABLE CURB PER WSDOT STD PLAN F–10.64–03. PAINT CURB COLOR SPECIFIED.
5. CONSTRUCT CONCRETE SIDEWALK PER DETAIL 2 ON SHEET DT01.
6. INSTALL WIDE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
7. INSTALL LANE EXTENSION LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
8. INSTALL RAISED PAVEMENT MARKERS PER CITY OF SAMMAMISH STD PLANS FIG04–03B AND FIG04–04. MATCH EXISTING PATTERN.
9. INSTALL PAVEMENT SYMBOLS (THRU ARROW, LEFT TURN ARROW AND RIGHT TURN ARROW) PER CITY OF SAMMAMISH STD PLAN FIG04–05.
10. REMOVE EXISTING 3–SECTION HEAD. FIELD DRILL AND WELD GALVANIZED TENON ON EXISTING SIGNAL POLE MAST ARM PER SIGNAL NOTES. INSTALL NEW 4–SECTION SIGNAL HEAD ON NEW TENON. NEW 4–SECTION HEAD SHALL HAVE LIGHT EMITTING DIODE (LED) DISPLAYS. BOTTOM HEAD SHALL BE A BIMODAL LED DISPLAY FOR NEW RIGHT–TURN OVERLAP MOVEMENT. INSTALL NEW FIVE CONDUCTOR (5C) BETWEEN NEW SIGNAL HEAD AND EXISTING TERMINAL CABINET.
11. INSTALL SIGN PER CITY OF SAMMAMISH STD PLAN FIG04–06 AND SIGN SCHEDULE.
12. INSTALL SIGN ON EXISTING LIGHT/SIGNAL POLE.
13. CONSTRUCT TYPE A CONCRETE FOUNDATION PER CITY WSDOT STD. PLAN J–28.30–03 AND PER MANUFACTURERS BOLT PATTERN. INSTALL CITY OF SAMMAMISH DECORATIVE POLE, MAST ARM, BASE, AND HPS LUMINAIRE HEAD PER DETAIL 3 ON DT01.
14. LOCATE AND INTERCEPT EXISTING ILLUMINATION CONDUIT. INSTALL NEW TYPE 1 JUNCTION BOX PER WSDOT STANDARD PLAN J–40.10–04. INSTALL NEW CONDUIT FROM JUNCTION BOX TO NEW LUMINAIRE. SPLICE NEW LUMINAIRE INTO EXISTING LIGHTING CIRCUIT. MAINTAIN EXISTING CIRCUITRY.
15. CONTRACTOR TO COORDINATE WITH CITY OF SAMMAMISH TRAFFIC ENGINEER AND KING COUNTY SIGNAL TECHNICIANS FOR SIGNAL TIMING UPGRADES ASSOCIATED WITH THE NEW WESTBOUND RIGHT–TURN OVERLAP MOVEMENT.
16. INSTALL 18–INCH YELLOW DELINEATION POST WITH REFLECTIVE TAPE.
17. INSTALL WHITE EDGE LINE PAVEMENT MARKINGS PER CITY OF SAMMAMISH STD PLAN FIG04–03A.
18. INSTALL CENTERLINE PER CITY OF SAMMAMISH STD PLAN FIG04–03A.



LOCATION 5

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

RENEE A. KONSTAR — STATE OF WASHINGTON — 25594 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

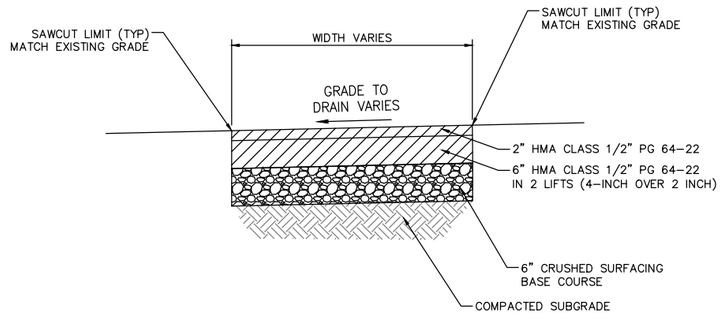
CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

ROADWAY PLAN

PL05

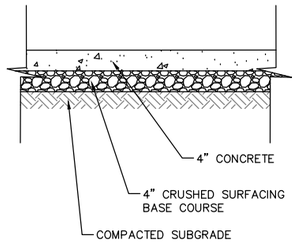
13 OF 17

By: RAKO Date: 2:29 PM File: P:\C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTRD001COSA0023_INT.dwg
Layout: PL05



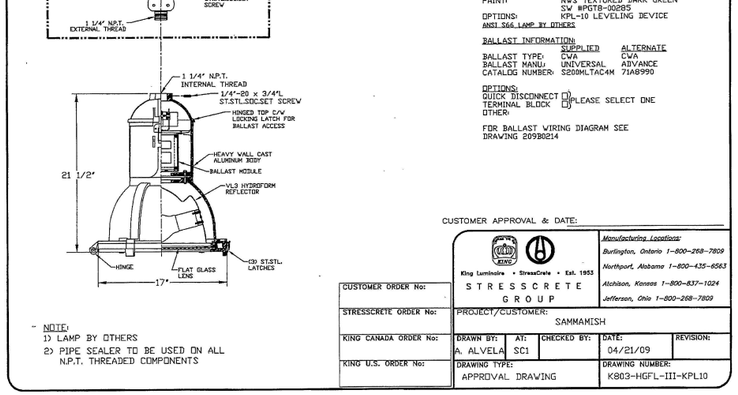
1 TYPICAL HMA PAVEMENT SECTION

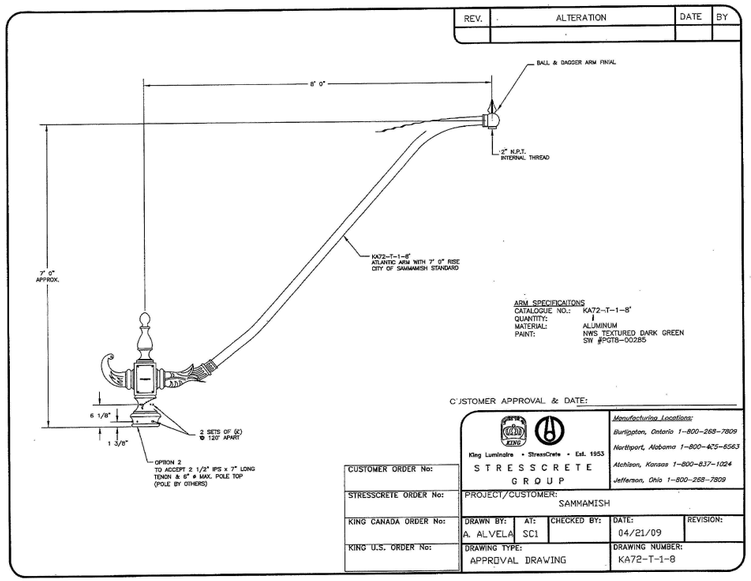
NOT TO SCALE

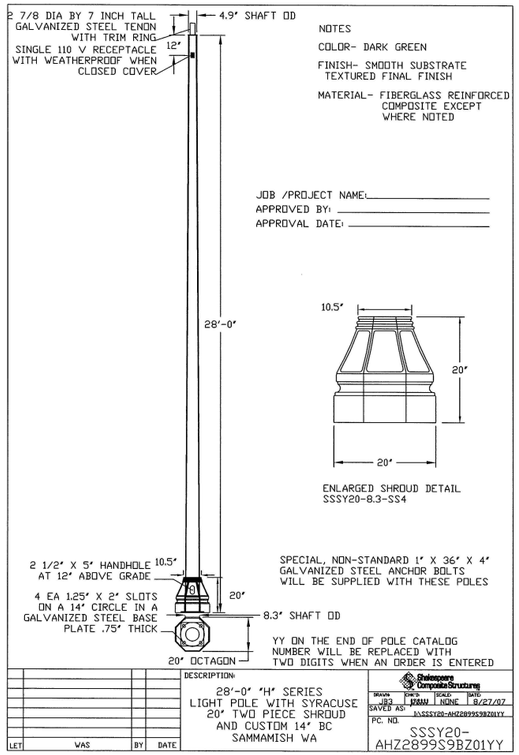


2 CONCRETE SIDEWALK SECTION

NOT TO SCALE







3 DECORATIVE LUMINAIRE DETAILS

NOT TO SCALE

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

NATHAN K. WOMACK — STATE OF WASHINGTON — 45913 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

TYPICAL SECTIONS AND DETAILS

DT01

14 OF 17

By: RAKO Date: 2:30 PM File: P:\C\COSA0000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTDT001COSA0023_INT.dwg
Layout: DT01

BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (feet)	155	200	250	305	360	425	495	570	645	730

TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R					
HOST VEHICLE WEIGHT 9,900 TO 22,000 lbs.			HOST VEHICLE WEIGHT > 22,000 lbs.		
< 45 MPH	45-55 MPH	> 55 MPH	< 45 MPH	45-55 MPH	> 55 MPH
100'	123'	172'	74'	100'	150'

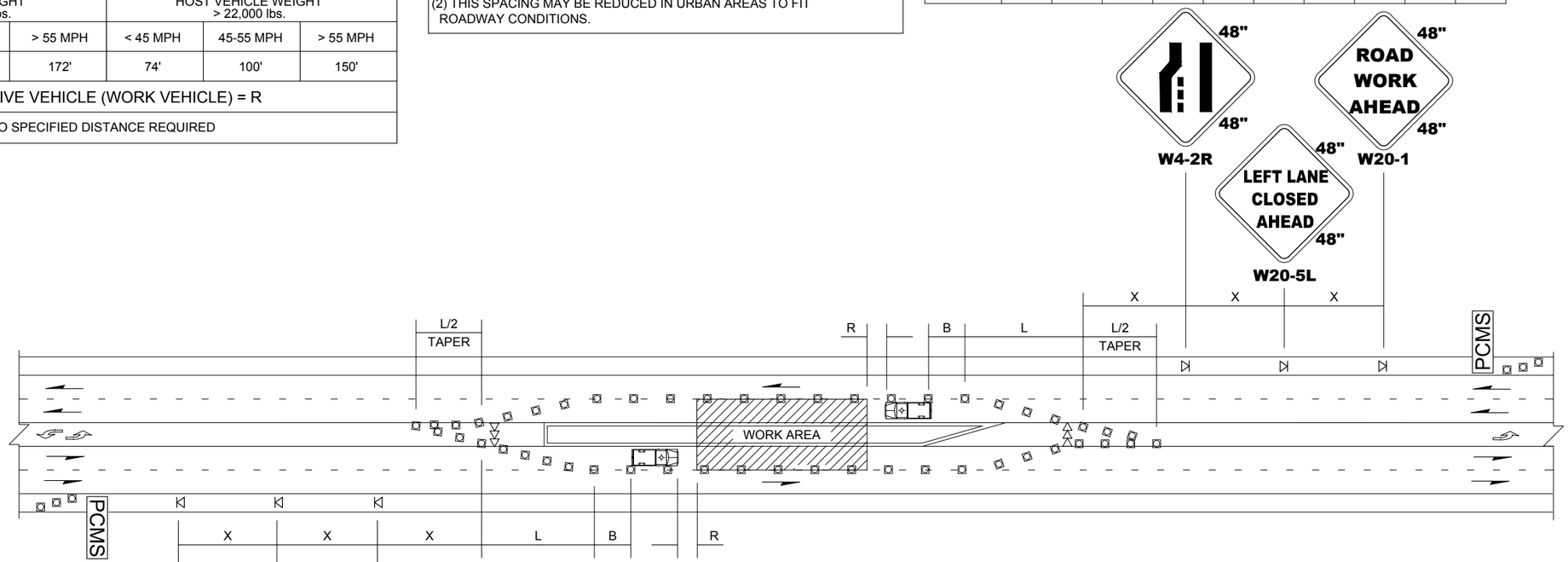
PROTECTIVE VEHICLE (WORK VEHICLE) = R
NO SPECIFIED DISTANCE REQUIRED

SIGN SPACING = X (1)		
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS & URBAN ARTERIALS RESIDENTAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ±(2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERCHANGE RAMPS, AT-GRADE INTERSECTIONS AND DRIVEWAYS.
(2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

MINIMUM TAPER LENGTH = L (feet)										
LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	-	-	-	-
11	115	165	225	295	495	550	-	-	-	-
12	125	180	245	320	540	600	-	-	-	-

CHANNELIZATION DEVICE SPACING (feet)		
MPH	TAPER	TANGENT
50	40	80
35/45	30	60
25/30	20	40



PCMS	
1	2
CENTER LANE CLOSED	NO LEFT TURNING
2.0 SEC	2.0 SEC

FIELD LOCATE IN ADVANCE OF TEMPORARY SIGNS.

LEFT LANE AND CENTER ISLAND CLOSURE FOR MULTI-LANE ROADWAYS
CLOSURE - 5 LANE ROADWAY

NOT TO SCALE

LEGEND

- TEMPORARY SIGN LOCATION
- CHANNELIZING DEVICES
- SEQUENTIAL ARROW SIGN
- PROTECTIVE VEHICLE
- PCMS — PORTABLE CHANGEABLE MESSAGE SIGN

NOTES

1. SEE SPECIAL PROVISIONS FOR WORK HOUR RESTRICTIONS.
2. ALL SIGNS ARE BLACK ON ORANGE.

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

RENEE A. KOESTER — STATE OF WASHINGTON — 53584 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY **WASHINGTON**

TRAFFIC CONTROL

TC01

By: RAKO Date: 2:30 PM File: P:\C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTTC001COSA0023_INT.dwg
Layout: TC01

MINIMUM LANE CLOSURE TAPER LENGTH = L (feet)										
LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	780	840

MINIMUM SHOULDER TAPER LENGTH = L/3 (feet)										
SHOULDER WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
8'	40	40	60	90	120	130	150	160	170	190
10'	40	60	90	90	150	170	190	200	220	240
USE A MINIMUM 3 DEVICES TAPER FOR SHOULDER LESS THEN 8'.										

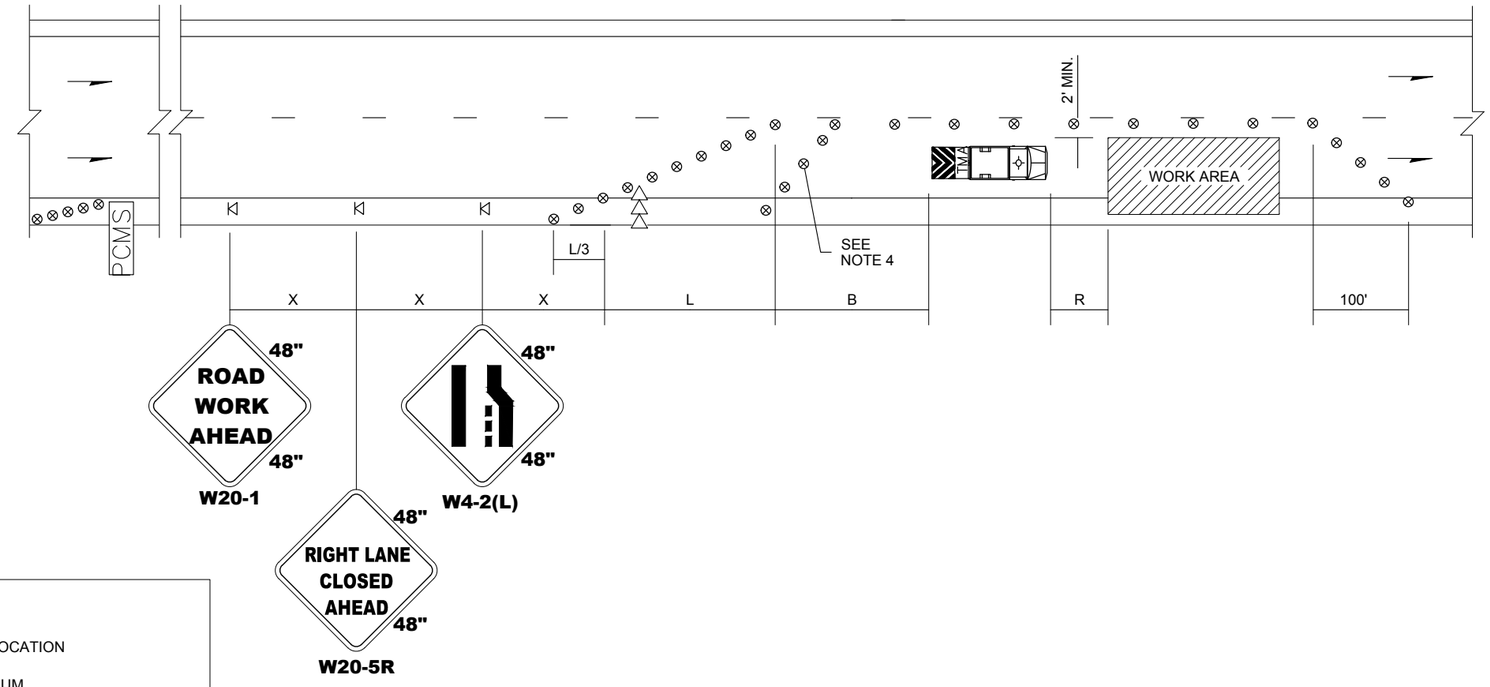
SIGN SPACING = X (1)		
FREEWAYS & EXPRESSWAYS	55 / 70 MPH	1500' ±
RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS & URBAN ARTERIALS RESIDENTIAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERCHANGE RAMPS, AT-GRADE INTERSECTIONS AND DRIVEWAYS.
(2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

CHANNELIZATION DEVICE SPACING (feet)		
MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (feet)	155	200	250	305	360	425	495	570	645	730

TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R					
HOST VEHICLE WEIGHT 9,900 TO 22,000 lbs.			HOST VEHICLE WEIGHT > 22,000 lbs.		
< 45 MPH	45-55 MPH	> 55 MPH	< 45 MPH	45-55 MPH	> 55 MPH
100'	123'	172'	74'	100'	150'



PCMS	
1	2
RIGHT LANE CLOSURE	1 MILE AHEAD
2.0 SEC	2.0 SEC

FIELD LOCATE 1 MILE ± IN ADVANCE OF LANE CLOSURE SIGNING.

LEGEND

- TEMPORARY SIGN LOCATION
- TRAFFIC SAFETY DRUM
- SEQUENTIAL ARROW SIGN
- TRANSPORTABLE ATTENUATOR
- PCMS — PORTABLE CHANGEABLE MESSAGE SIGN

SINGLE-LANE CLOSURE FOR MULTI-LANE ROADWAYS

NOT TO SCALE

NOTES:

1. SEE SPECIAL PROVISIONS FOR WORK HOUR RESTRICTIONS.
2. EXTEND DEVICE TAPER AT L/3 ACROSS SHOULDER.
3. DEVICES SHALL NOT ENCROACH INTO THE ADJACENT LANE.
4. USE TRANSVERSE DEVICES IN CLOSED LANE EVERY 1000' (FT) (RECOMMENDED).
5. DEVICE SPACING FOR THE DOWNSTREAM TAPER SHALL BE 20' (FT).
6. ALL SIGNS ARE BLACK ON ORANGE.

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

RENEE A. KOESTER — STATE OF WASHINGTON — 55594 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY WASHINGTON

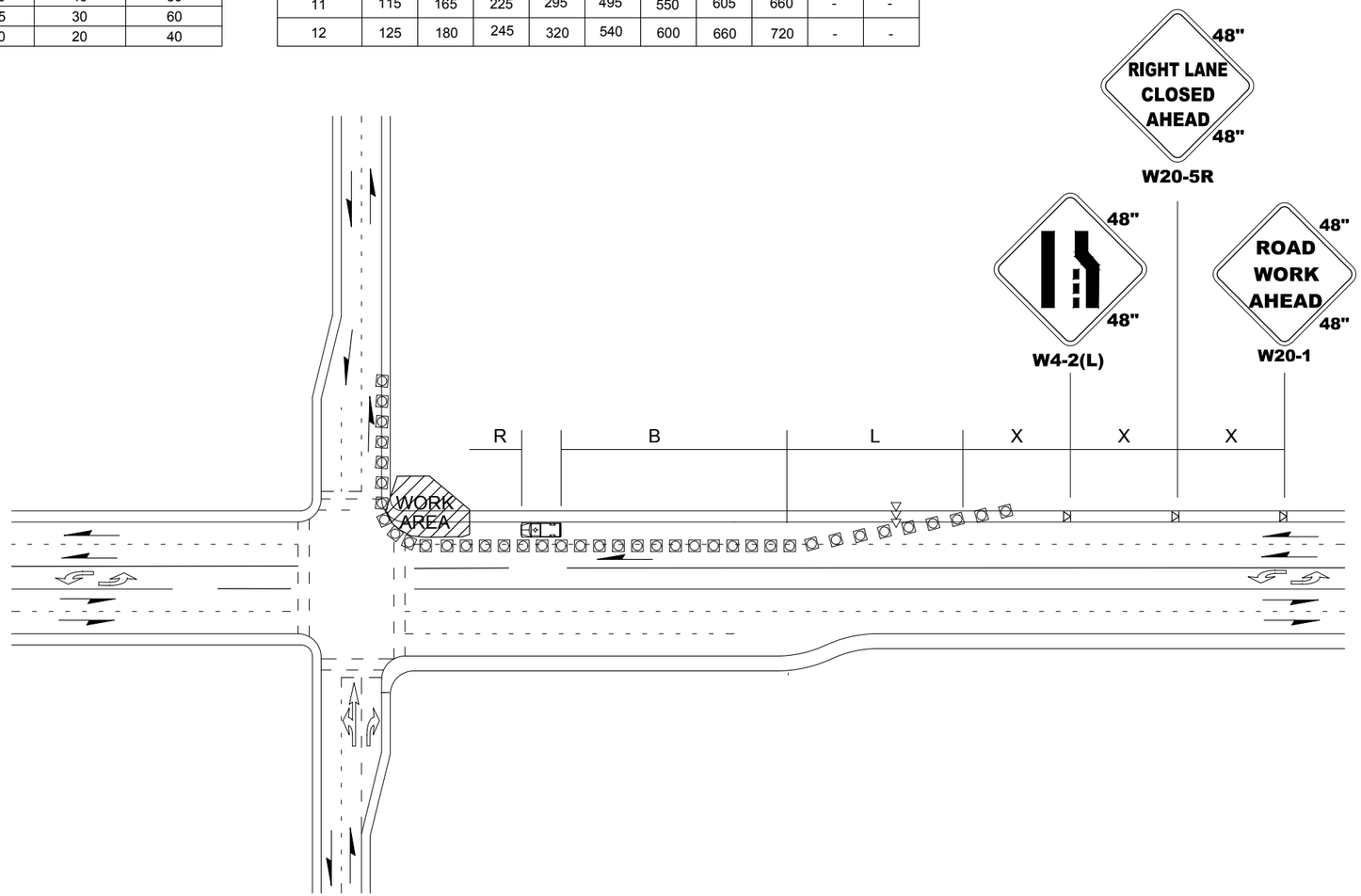
TRAFFIC CONTROL

TC02

By: RAKO Date: 2:30 PM File: P:\C\COSA00000023\0400CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTTC001COSA0023_INT.dwg Layout: TC02

CHANNELIZATION DEVICE SPACING (feet)		
MPH	TAPER	TANGENT
50/70	40	80
35/45	30	60
25/30	20	40

MINIMUM TAPER LENGTH = L (feet)										
LANE WIDTH (feet)	Posted Speed (mph)									
	25	30	35	40	45	50	55	60	65	70
10	105	150	205	270	450	500	550	-	-	-
11	115	165	225	295	495	550	605	660	-	-
12	125	180	245	320	540	600	660	720	-	-



BUFFER DATA										
LONGITUDINAL BUFFER SPACE = B										
SPEED (MPH)	25	30	35	40	45	50	55	60	65	70
LENGTH (feet)	155	200	250	305	360	425	495	570	645	730

TRANSPORTABLE ATTENUATOR ROLL AHEAD DISTANCE = R					
HOST VEHICLE WEIGHT 9,900 TO 22,000 lbs.			HOST VEHICLE WEIGHT > 22,000 lbs.		
< 45 MPH	45-55 MPH	> 55 MPH	< 45 MPH	45-55 MPH	> 55 MPH
100'	123'	172'	74'	100'	150'

PROTECTIVE VEHICLE (WORK VEHICLE) = R
NO SPECIFIED DISTANCE REQUIRED

SIGN SPACING = X (1)		
RURAL HIGHWAYS	60 / 65 MPH	800' ±
RURAL ROADS	45 / 55 MPH	500' ±
RURAL ROADS & URBAN ARTERIALS	35 / 40 MPH	350' ±
RURAL ROADS & URBAN ARTERIALS RESIDENTAL & BUSINESS DISTRICTS	25 / 30 MPH	200' ± (2)
URBAN STREETS	25 MPH OR LESS	100' ± (2)

(1) ALL SPACING MAY BE ADJUSTED TO ACCOMMODATE INTERCHANGE RAMPS, AT-GRADE INTERSECTIONS AND DRIVEWAYS.
(2) THIS SPACING MAY BE REDUCED IN URBAN AREAS TO FIT ROADWAY CONDITIONS.

LEGEND

- TEMPORARY SIGN LOCATION
- CHANNELIZING DEVICES
- SEQUENTIAL ARROW SIGN
- PROTECTIVE VEHICLE - RECOMMENDED

INTERSECTION LANE CLOSURE ~ FIVE LANE ROADWAY

NOT TO SCALE

NOTES

1. RECOMMEND EXTENDING DEVICE TAPER (L/3) ACROSS SHOULDER.
2. IF A SIGNAL IS PRESENT, IT SHALL BE SET TO "RED FLASH MODE" OR TURNED OFF DURING FLAGGING OPERATIONS.
3. MAINTAIN A MINIMUM OF ONE ACCESS POINT FOR EACH BUSINESS WITHIN WORK AREA LIMITS.
4. ALL SIGNS ARE BLACK ON ORANGE.

CALL 48 HOURS BEFORE YOU DIG 1-800-424-5555

NO.	DATE	BY	APPR	REVISION

JOB#: COSA0023
DSGN: NKW/RAKO
DRN: OXA
CHKD: KAHA
DATE: 6/21/18
SCALE: AS NOTED

DAVID EVANS AND ASSOCIATES INC.
14432 SE Eastgate Way, Suite 400
Bellevue Washington 98007
Phone: 425.519.6500

City of Sammamish Washington

RENEE A. KOESTER — STATE OF WASHINGTON — 55594 — REGISTERED PROFESSIONAL ENGINEER — 6/21/18

CITY OF SAMMAMISH
2018 INTERSECTION IMPROVEMENTS PROJECT
KING COUNTY **WASHINGTON**

TRAFFIC CONTROL

TC03

By: RAKO Date: 2:30 PM File:P: \C\COSA00000023\04000CAD\SHEETS\INTERSECTION IMPROVEMENTS\TTTC001COSA0023_INT.dwg
Layout: TC03